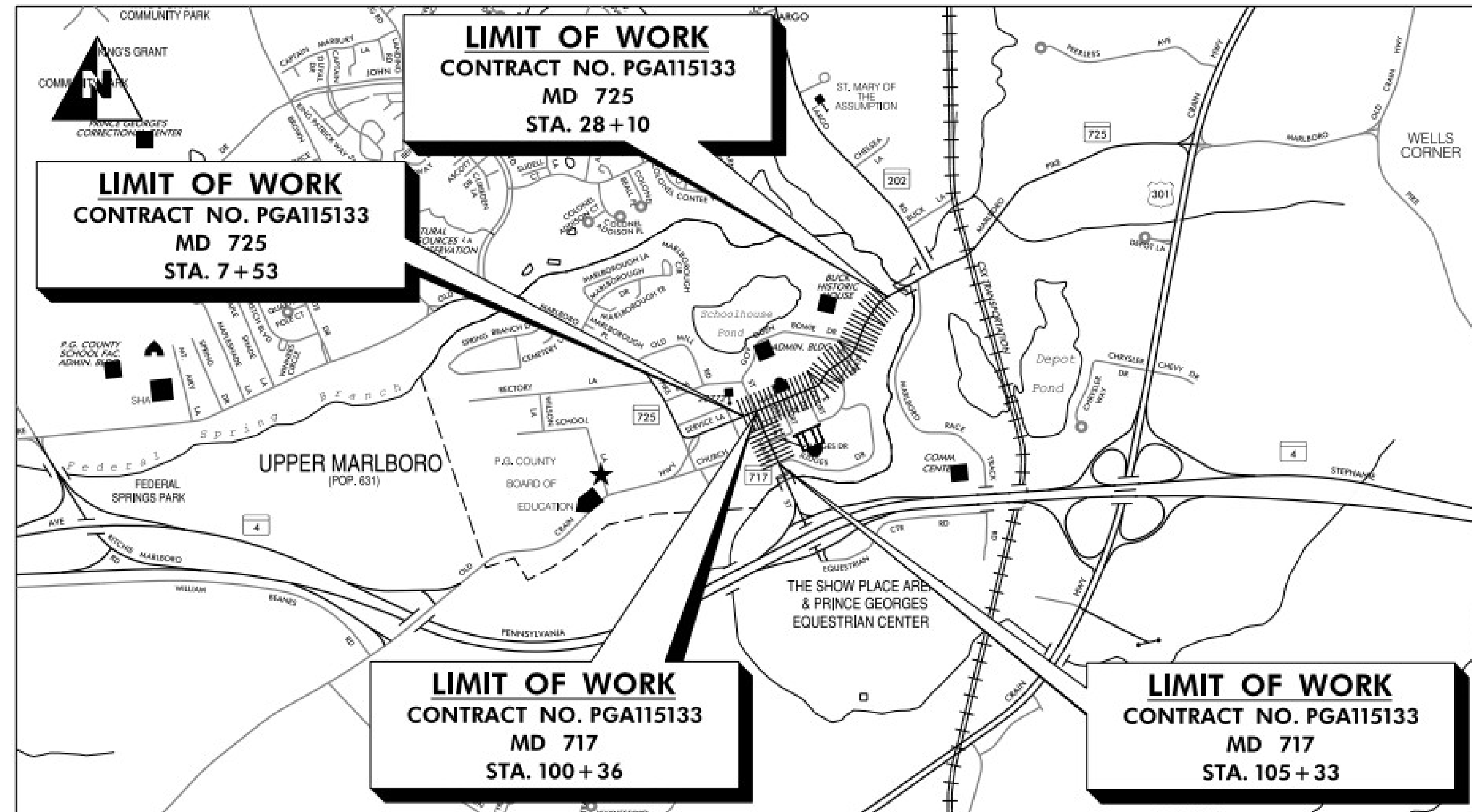


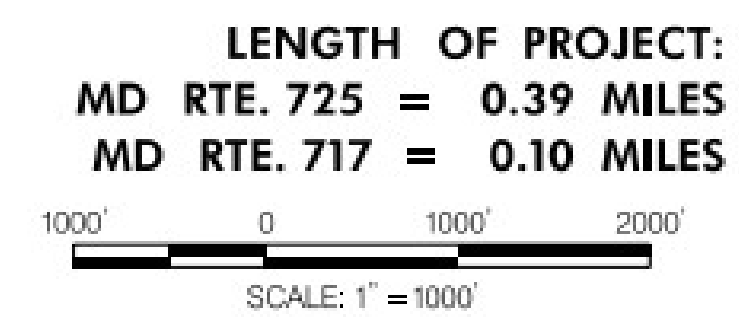
MDOT MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

S.H.A. CONTRACT NO. – PGA115133
FEDERAL AID PROJECT NO. – AC-STBG-1194(5)E

**MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE
TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS**



UPPER MARLBORO
PRINCE GEORGE'S COUNTY



GEOMETRIC DESIGN CRITERIA
THIS PROJECT WAS DESIGNED IN ACCORDANCE WITH THE 2018 PUBLICATION OF AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS," STANDARD SPECIFICATIONS BOOK, BOOK OF STANDARDS AND MARYLAND MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MDMUTCD). ALL WORK ON THIS PROJECT SHALL CONFORM TO: THE LATEST APPROVED MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION (MDOT SHA) "STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS" REVISIONS THEREOF OR ADDITIONS THERETO, AS INDICATED IN THE PROJECT DESCRIPTION OF THE INVITATION FOR BIDS BOOK; THE SPECIAL PROVISIONS INCLUDED IN THE INVITATION FOR BIDS BOOK; THE ADMINISTRATION'S "BOOK OF STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES" AND THE LATEST VERSION OF MDMUTCD.

RIGHT OF WAY
RIGHT OF WAY AND EASEMENT LINES SHOWN ON THESE PLANS ARE FOR ASSISTANCE IN INTERPRETING THE PLANS. THEY ARE NOT OFFICIAL. FOR OFFICIAL FEE RIGHT OF WAY AND EASEMENT INFORMATION, SEE APPROPRIATE RIGHT OF WAY PLATS.

UTILITIES
THE LOCATION OF UTILITIES SHOWN ON THE PLANS ARE FOR INFORMATION AND GUIDANCE ONLY. NO GUARANTEE IS MADE OF THE ACCURACY OF SAID LOCATIONS.

ADA COMPLIANCE
THE DESIGN OF THIS PROJECT HAS INCORPORATED FACILITIES TO ACCOMMODATE PERSONS WITH DISABILITIES IN COMPLIANCE WITH STATE AND FEDERAL REQUIREMENTS.

ENVIRONMENTAL INFORMATION
ALL STORMWATER MANAGEMENT FACILITIES CONSTRUCTED FOR THIS CONTRACT SHALL BE INSPECTED AND MAINTAINED IN ACCORDANCE WITH THE MDOT SHA BEST MANAGEMENT PRACTICES (BMP) INSPECTION AND REMEDIATION PROGRAM.

STANDARD STABILIZATION NOTE:
FOLLOWING INITIAL SOIL DISTURBANCE OR REDISTURBANCE, PERMANENT OR TEMPORARY STABILIZATION SHALL BE COMPLETED WITHIN THREE (3) CALENDAR DAYS AS TO THE SURFACE OF ALL PERIMETER CONTROLS, DIKES, SWALES, DITCHES, PERIMETER SLOPES, AND ALL SLOPES GREATER THAN 3 HORIZONTAL TO 1 VERTICAL (3:1), AND SEVEN DAYS (7) AS TO ALL OTHER DISTURBED OR GRADED AREAS ON THE PROJECT SITE.

OWNERS / DEVELOPERS CERTIFICATION:
I/ WE HEREBY CERTIFY THAT ANY CLEARING, GRADING, CONSTRUCTION AND/OR DEVELOPMENT WILL BE DONE PURSUANT TO THIS PLAN, AND THAT ANY RESPONSIBLE PERSONNEL INVOLVED IN THE CONSTRUCTION PROJECT WILL HAVE A CERTIFICATE OF ATTENDANCE AT A MARYLAND DEPARTMENT OF THE ENVIRONMENT (MDE) APPROVED TRAINING PROGRAM FOR THE CONTROL OF SEDIMENT AND EROSION BEFORE BEGINNING THE PROJECT. I/HEREBY AUTHORIZE THE RIGHT OF ENTRY FOR PERIODIC ON-SITE EVALUATION BY MDE COMPLIANCE INSPECTORS.

EXISTING STRUCTURES PLANS
FOR THE CONVENIENCE AND INFORMATION OF BIDDERS, PRINTS OF PLANS OF EXISTING PERTINENT STRUCTURE(S) ARE INCLUDED WITH THIS CONTRACT. NO RESPONSIBILITY FOR THEIR ACCURACY OR COMPLETENESS IS ASSUMED BY THE MDOT SHA. DIMENSIONS, DETAILS, ETC., AS SHOWN THEREON MAY NOT BE AS BUILT.

<p>STORMWATER AND SEDIMENT CONTROL APPROVAL</p> <p><i>Walter Han</i> 3/3/2022 APPROVED DATE</p> <p>DIVISION CHIEF, PLAN REVIEW DIVISION</p> <p>PRD NO: 20-PR-0006-03 EXPIRATION DATE: 03/02/2025</p>	<p>MODIFICATIONS</p> <p>Mod 1 (PreAd) 03/31/2022</p>
--	---

<p>RJM CONSULTING ENGINEERS 6031 UNIVERSITY BLVD, SUITE 290 ELLCOTT CITY, MARYLAND TEL: 410.730.1001 FAX: 410.730.5403</p>	
<p>SHEET NOS. AND OTHER CLARIFICATIONS: 14</p>	
<p>I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. MD LICENSE NO. 22051 EXPIRATION DATE: 7/25/2022</p>	
<p>WRA Whitman, Requardt & Associates, LLP Engineers · Architects · Environmental Planners Est. 1915</p>	
<p>SHEET NOS. AND OTHER CLARIFICATIONS: 1-13, 15-29</p>	
<p>I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. MD LICENSE NO. 23143 EXPIRATION DATE: 6/25/2023</p>	

REVISIONS
NOTE: SEE SHEET NO. 2 FOR LIST OF REVISED SHEET NUMBERS

<p><i>[Signature]</i> APPROVED DIVISION CHIEF, INNOVATIVE CONTRACTING DIVISION</p>	<p>03/01/2022 DATE</p>
<p><i>[Signature]</i> APPROVED DIRECTOR, OFFICE OF HIGHWAY DEVELOPMENT</p>	<p>03/01/2022 DATE</p>
<p><i>[Signature]</i> APPROVED ACTING DEPUTY ADMINISTRATOR / CHIEF ENGINEER FOR PLANNING, ENGINEERING, REAL ESTATE AND ENVIRONMENT</p>	<p>03/01/2022 DATE</p>

GENERAL NOTES

- BASELINE SHOWN FOR ILLUSTRATIVE PURPOSES.
- DIMENSIONS AND STATIONS ARE APPROXIMATE.
- ALL SIDEWALK AND RAMPS SHALL BE A MINIMUM OF 60" WIDE, TAPER SIDEWALK TO EXISTING IN ACCORDANCE TO STD. NO 655.02 UNLESS NOTED OTHERWISE. SIDEWALK CROSS SLOPE SHALL BE 2% MAXIMUM WHERE THE SIDEWALK TRANSITIONS TO EXISTING, THE CROSS SLOPE SHALL TRANSITION TO EXISTING OVER 5'.
- ALL TRANSITION PANELS SHALL BE TO THE NEAREST SIDEWALK JOINT (5' MIN.).
- THE CONTRACTOR MAY ADJUST THE CROSS SLOPE OF THE SIDEWALK TO MAINTAIN THE POSITIVE SHEET FLOW USING A 2% MAXIMUM SIDEWALK SLOPE.
- MATCH EXISTING ELEVATIONS AT THE EDGE OF PROPOSED SIDEWALK, SIDEWALK RAMPS AND CURB AND GUTTER. MAXIMUM VERTICAL ELEVATION BETWEEN ADJACENT SURFACES NOT TO EXCEED 1/4".
- SIDEWALK RECONSTRUCTION LIMITS SHALL COINCIDE WITH CRACK CONTROL JOINTS (JOINT GROOVES) OR EXPANSION JOINTS IN EXISTING SIDEWALK. NEW EXPANSION JOINTS SHALL BE PROVIDED AT THE INTERFACE OF EXISTING AND PROPOSED SIDEWALK. SEE MDOT SHA STANDARD MD-655.01.
- FORM NEW SIDEWALK AND EXPANSION JOINT MATERIAL AROUND EXISTING UTILITY POLES MANHOLE COVERS, VALVE COVERS AND FIRE HYDRANTS AS NECESSARY.
- INSTALL DETECTABLE WARNING SURFACE ON SIDEWALK RAMPS AS PER MDOT SHA STANDARD MD-655.40.
- ALL DRIVEWAY ENTRANCES THAT HAVE CURB RETURNS MUST CARRY A MINIMUM 36" WIDE PEDESTRIAN PATHWAY WITH A MAXIMUM 2% CROSS-SLOPE ACROSS THE ENTIRE ENTRANCE REGARDLESS OF WHAT MATERIAL IS USED TO CONSTRUCT THE ENTRANCE.
- ALL DISTURBED AREAS WITHIN THE LOD SHALL RECEIVE 4" TOPSOIL AND STABILIZED WITH TURFGRASS SOD ESTABLISHMENT UNLESS OTHERWISE NOTED.
- RESET EXISTING SIGNS AND/OR RELOCATE SIGNS ON NEW POSTS AS SHOWN ON THE PLANS TO BE DISTURBED BY THE CONSTRUCTION AND AS DIRECTED BY THE ENGINEER.
- ADJUST EXISTING UTILITIES AS REQUIRED.
- THE CONTRACTOR SHALL REPLACE EXISTING DAMAGED CURB WHICH IS ADJACENT TO LOCATIONS WHERE CURB RAMPS ARE BEING RECONSTRUCTED OR INSTALLED.
- EXISTING CURB HEIGHTS PROVIDED ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR. RAMPS AND DRIVEWAY TRANSITION LENGTHS SHALL BE VERIFIED IN THE FIELD ACCORDING TO MDOT SHA STANDARDS MD-630.01, MD-630.02, MD-655.11, MD-655.12 AND MD 655.13.
- ALL CURB REPLACEMENTS TERMINAL ADJACENT TO PARKING LOT DRIVEWAY ENTRANCES SHALL BE CONSTRUCTED NOSE DOWN TO MATCH EXISTING GRADE OR AS DIRECTED BY THE ENGINEER.
- EXISTING INLETS SHALL NOT BE DAMAGED DURING CONSTRUCTION. CONTRACTOR SHALL REPLACE INLETS DAMAGED BY THEIR ACTIVITIES AT NO ADDITIONAL COST TO THE ADMINISTRATION.
- EXISTING TREES, SHRUBS AND THEIR RESPECTIVE ROOT SYSTEMS SHALL NOT BE DISTURBED DURING CONSTRUCTION UNLESS OTHERWISE NOTED.
- PAVEMENT MARKINGS SHOWN ARE EXISTING UNLESS OTHERWISE NOTED TO BE REPLACED.
- NO DISTURBED AREA SHALL BE LEFT UNSTABILIZED OVERNIGHT.
- SEDIMENT AND EROSION CONTROL REGULATIONS WILL BE STRICTLY ENFORCED DURING CONSTRUCTION.
- ALL NEW AND UPGRADED EXISTING CROSSWALKS SHALL FOLLOW MDOT SHA SHELF TYPICAL 550.02.
- THE CONTRACTOR SHALL REUSE THE EXISTING GRADED AGGREGATE BASE UNDER THE DRIVEWAY ENTRANCE IN ORDER TO SATISFY THE REQUIREMENTS FOR GRADED AGGREGATE PLACEMENT OUTLINED IN MDOT SHA STANDARDS MD-630.01, MD-630.02, AND MD-630.03.
- THE CONTRACTOR SHALL MAINTAIN ADA COMPLIANT PEDESTRIAN ACCESS TO THE ADJACENT PROPERTIES AS DIRECTED BY THE ENGINEER. THE COST IS INCIDENTAL TO MOT ITEMS.
- SAW CUTTING IS INCIDENTAL TO THE PERTINENT PAY ITEM.

INDEX OF SHEETS

SHEET NO.	DWG. NO.	DESCRIPTION
1	TI-01	TITLE SHEET
2	GI-01	INDEX OF SHEETS AND GENERAL NOTES
3-13	PS-01 TO PS-11	ROADWAY PLAN SHEETS
14	ES-01	EROSION AND SEDIMENT CONTROL GENERAL NOTES
15	SN-1	GENERAL NOTES AND PROPOSALS
16-26	SN-2.01 TO SN-2.11	SIGNING AND MARKING PLAN SHEETS
27	SN-11.01	INDEX OF QUANTITIES
28	SG-01	TRAFFIC SIGNALIZATION PLAN
29	SG-02	GENERAL INFORMATION SHEET

MAINTENANCE OF TRAFFIC NOTES:

- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE MDOT SHA STANDARD NUMBERS MD 104.00-01 THRU 104.06-27.
- NO WORK IS TO BEGIN UNTIL TEMPORARY TRAFFIC CONTROL DEVICES INCLUDING, BUT NOT LIMITED TO, ADVANCED WARNING SIGNS, DRUMS, ARROW PANELS, ETC., ARE IN PLACE AND OPERATIONAL.
- ADVANCED NOTIFICATION OF SIDEWALK CLOSURES SHALL BE PROVIDED. A BARRIER THAT IS DETECTABLE BY A PERSON WITH A VISUAL DISABILITY SHALL BE PLACED ACROSS THE FULL WIDTH OF SIDEWALK.
- WHERE PEDESTRIANS ARE DIRECTED TO CROSS THE OPPOSITE SIDE OF THE ROADWAY, APPROPRIATE ADVANCED SIGNING SHOULD BE PLACED AT THE INTERSECTIONS RATHER THAN MID-BLOCK CROSSINGS.
- TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY PATHWAY OR OTHER PEDESTRIAN FACILITY.
- PROVIDE ACCESS TO ALL ENTRANCES AND COORDINATE DRIVEWAY ACCESS WITH ADJACENT PROPERTY OWNERS.
- WHERE SIDEWALK IS PRESENT ON BOTH SIDES OF THE STREET, ONE SIDE SHALL REMAIN OPEN.
- CONTRACTOR SHALL START AND COMPLETE ONE BLOCK ON ONE SIDE OF THE ROADWAY AT A TIME BEFORE PROCEEDING TO THE NEXT BLOCK IN ORDER TO MINIMIZE IMPACTS ON PEDESTRIAN TRAFFIC AND ON-STREET PARKING. CLOSING EXISTING ON-STREET PARKING TO THE PUBLIC SHALL BE LIMITED TO THE ONE BLOCK WORK AREA. CLOSED PARKING LANES MAY BE USED FOR TEMPORARY TRAFFIC CONTROL.
- CONTRACTOR SHALL FOLLOW THE REQUIREMENTS FOR TEMPORARY LANE AND SHOULDER CLOSURES AS SET FORTH IN THE TRAFFIC CONTROL PLAN SPECIAL PROVISION 104.01.

SEQUENCE OF CONSTRUCTION

GENERAL NOTES

- THE CONTRACTOR SHALL NOTIFY THE MDOT SHA QUALITY ASSURANCE DIVISION (QAD) AT (410) 365-0164 A MINIMUM OF SEVEN (7) DAYS PRIOR TO ANY EARTH DISTURBANCE TO SET UP A PRE-CONSTRUCTION MEETING BETWEEN THE PROJECT REPRESENTATIVES AND THE QAD.
- CONSTRUCT SIDEWALK/RAMP/DRIVEWAY IMPROVEMENTS AND PERMANENTLY STABILIZE ALL DISTURBED AREAS FOLLOWING THE COMPLETION OF THE PROPOSED IMPROVEMENT. USE SAME-DAY STABILIZATION PRACTICES FOR THIS PROJECT. IN ORDER TO COMPLY WITH THE SAME-DAY STABILIZATION PRACTICES, NO DISTURBED AREAS SHALL BE LEFT UNSTABILIZED AT THE END OF ANY WORK DAY.
- UPON COMPLETION OF IMPROVEMENTS AND PERMANENT STABILIZATION, NOTIFY THE QAD TO OBTAIN FINAL ACCEPTANCE BY THE ADMINISTRATION.

BEST MANAGEMENT PRACTICES FOR WORKING IN NONTIDAL WETLANDS, WETLAND BUFFERS, WATERWAYS, AND 100-YEAR FLOODPLAINS

- NO EXCESS FILL, CONSTRUCTION MATERIAL, OR DEBRIS SHALL BE STOCKPILED OR STORED IN NONTIDAL WETLANDS, NONTIDAL WETLAND BUFFERS, WATERWAYS, OR THE 100-YEAR FLOODPLAIN.
- PLACE MATERIALS IN A LOCATION AND MANNER WHICH DOES NOT ADVERSELY IMPACT SURFACE OR SUBSURFACE WATER FLOW INTO OR OUT OF NONTIDAL WETLANDS, NONTIDAL WETLAND BUFFERS, WATERWAYS, OR THE 100-YEAR FLOODPLAIN.
- DO NOT USE THE EXCAVATED MATERIAL AS BACKFILL IF IT CONTAINS WASTE METAL PRODUCTS, UNSIGHTLY DEBRIS, TOXIC MATERIAL, OR ANY OTHER DELETERIOUS SUBSTANCE. IF ADDITIONAL BACKFILL IS REQUIRED, USE CLEAN MATERIAL FREE OF WASTE METAL PRODUCTS, UNSIGHTLY DEBRIS, TOXIC MATERIAL, OR ANY OTHER DELETERIOUS SUBSTANCE.
- PLACE HEAVY EQUIPMENT ON MATS OR SUITABLY OPERATE THE EQUIPMENT TO PREVENT DAMAGE TO NONTIDAL WETLANDS, NONTIDAL WETLAND BUFFERS, WATERWAYS, OR THE 100-YEAR FLOODPLAIN.
- REPAIR AND MAINTAIN ANY SERVICEABLE STRUCTURE OR FILL SO THERE IS NO PERMANENT LOSS OF NONTIDAL WETLANDS, NONTIDAL WETLAND BUFFERS, OR WATERWAYS, OR PERMANENT MODIFICATION OF THE 100-YEAR FLOODPLAIN IN EXCESS OF THAT LOST UNDER THE ORIGINALLY AUTHORIZED STRUCTURE OR FILL.
- RECTIFY ANY NONTIDAL WETLANDS, WETLAND BUFFERS, WATERWAYS, OR 100-YEAR FLOODPLAIN TEMPORARILY IMPACTED BY ANY CONSTRUCTION.
- ALL STABILIZATION IN THE NONTIDAL WETLAND AND NONTIDAL WETLAND BUFFER SHALL CONSIST OF THE FOLLOWING SPECIES: ANNUAL RYEGRASS (LOLIUM MULTIFLORUM), MILLET (SETARIA ITALICA), BARLEY (HORDEUM SP.), OATS (UNIOLA SP.), AND/OR RYE (SECALE CEREALE). THESE SPECIES WILL ALLOW FOR THE STABILIZATION OF THE SITE WHILE ALSO ALLOWING FOR THE VOLUNTARY REVEGETATION OF NATURAL WETLAND SPECIES. OTHER NON-PERSISTENT VEGETATION MAY BE ACCEPTABLE, BUT MUST BE APPROVED BY THE NONTIDAL WETLANDS AND WATERWAYS DIVISION. KENTUCKY 31 FESCUE SHALL NOT BE UTILIZED IN WETLAND OR BUFFER AREAS. THE AREA SHOULD BE SEEDED AND MULCHED TO REDUCE EROSION AFTER CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED.
- AFTER INSTALLATION HAS BEEN COMPLETED, MAKE POST-CONSTRUCTION GRADES AND ELEVATIONS THE SAME AS THE ORIGINAL GRADES AND ELEVATIONS IN TEMPORARILY IMPACTED AREAS.
- TO PROTECT AQUATIC SPECIES, IN-STREAM WORK IS PROHIBITED AS DETERMINED BY THE CLASSIFICATION OF THE STREAM: USE IV WATERS: IN-STREAM WORK SHALL NOT BE CONDUCTED DURING THE PERIOD MARCH 1 THROUGH MAY 31, INCLUSIVE, DURING ANY YEAR.
- STORMWATER RUNOFF FROM IMPERVIOUS SURFACES SHALL BE CONTROLLED TO PREVENT THE WASHING OF DEBRIS INTO THE WATERWAY.
- CULVERTS SHALL BE CONSTRUCTED AND ANY RIPRAP PLACED SO AS NOT TO OBSTRUCT THE MOVEMENT OF AQUATIC SPECIES, UNLESS THE PURPOSE OF THE ACTIVITY IS TO IMPOUND WATER.

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL MODIFICATION 1 APPROVAL	
<i>Matthew Nam</i>	3/3/2022
Chief, Plan Review Division	Expiration Date: 03/02/2025
PRD No. 20-PR-0006-03	
MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL FINAL APPROVAL	
<i>Matthew Nam</i>	3/3/2022
Chief, Plan Review Division	Expiration Date: 03/02/2025
PRD No. 20-PR-0006-03	

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

INNOVATIVE CONTRACTING DIVISION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

INDEX OF SHEETS AND GENERAL NOTES

SCALE	NTS	DATE	APRIL 12, 2022	CONTRACT NO.	PGAT15133
DESIGNED BY	HWP	COUNTY	PRINCE GEORGE'S		
DRAWN BY	SAZ	LOGMILE			
CHECKED BY	REH	HORIZONTAL SCALE	N/A		
MDE/PRD	20-PR-0006-03	VERTICAL SCALE	N/A		
DRAWING NO.	GI-01	OF	01	SHEET NO.	2 OF 29

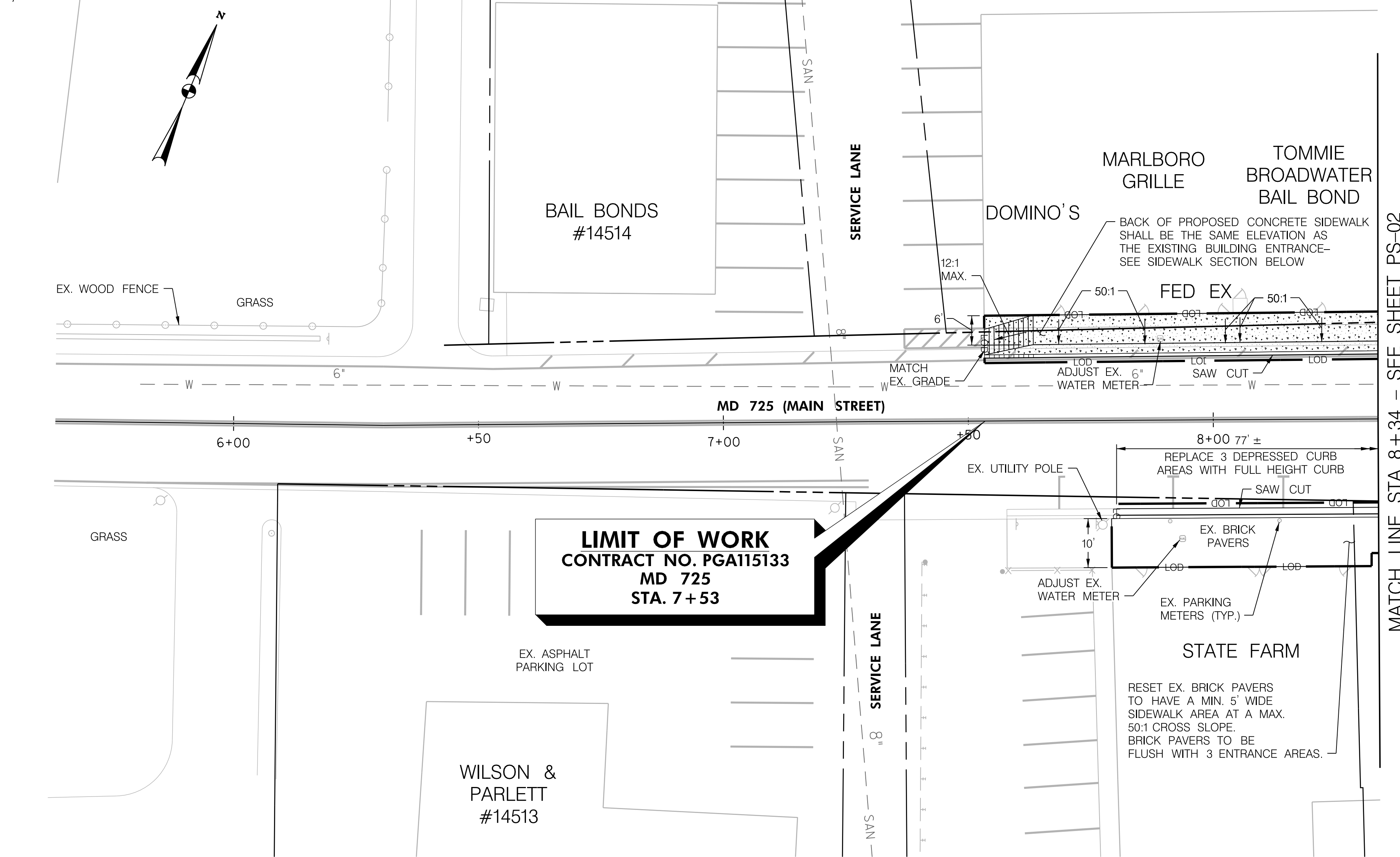


BY: mknowles

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
FINAL APPROVAL
3/3/2022
Chief, Plan Review Division
PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
MODIFICATION 1 APPROVAL
3/3/2022
Chief, Plan Review Division
PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

E 1382600
N 418700



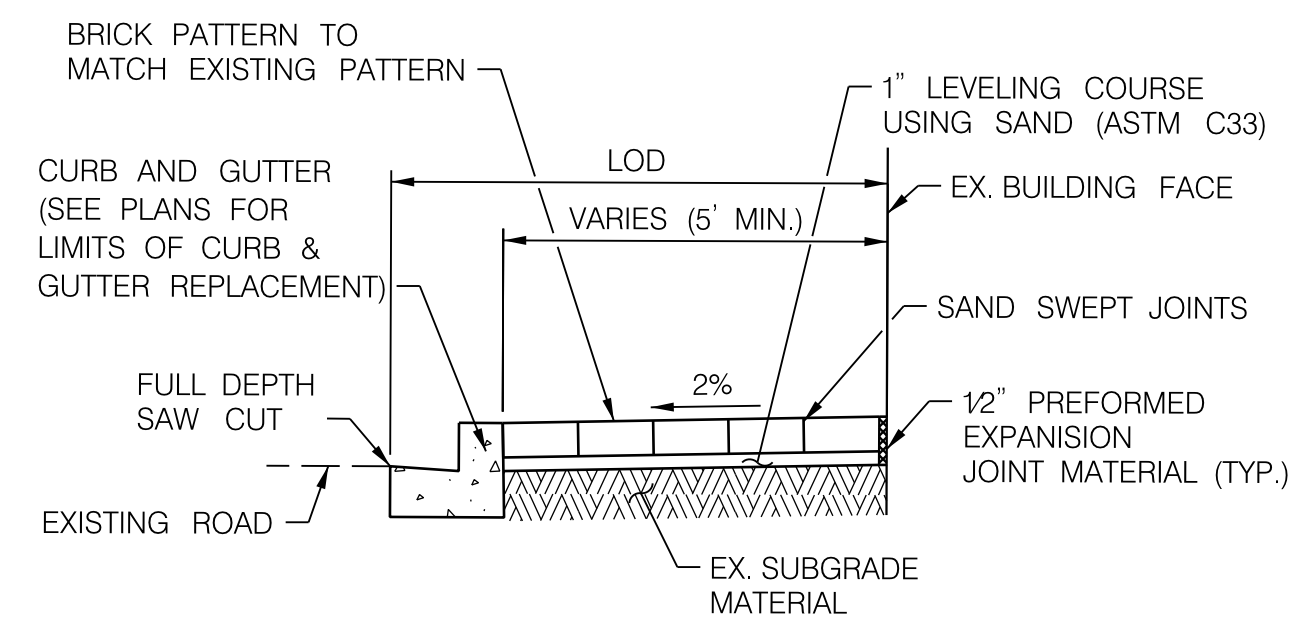
ADJUST WATER METER FRAME AND COVER			
Q	STA. & OFFSET	EA	REMARKS
MD 725	7+89, LT.	1	
MD 725	7+93, RT.	1	

5 INCH CONCRETE SIDEWALK			
Q	STA. & OFFSET	SF	REMARKS
MD 725	7+53 TO 8+34, LT.	643	

TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH			
Q	STA. & OFFSET	LF	REMARKS
MD 725	7+80 TO 8+34, RT.	53	

TYPE A CURB ANY HEIGHT OR DEPTH			
Q	STA. & OFFSET	LF	REMARKS
MD 725	7+53 TO 8+34, LT.	80	

REMOVE AND RESET BRICK SIDEWALK			
Q	STA. & OFFSET	SF	REMARKS
MD 725	7+79 TO 8+34, RT.	539	



REMOVE AND RESET BRICK SIDEWALK
NOT TO SCALE

NOTE 1: INSTALLATION OF EX. BRICKS, LEVELING COURSE, AND EXPANSION JOINT MATERIALS ARE INCIDENTAL TO THE REMOVAL AND RESETTING OF THE EX. BRICKS.

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.

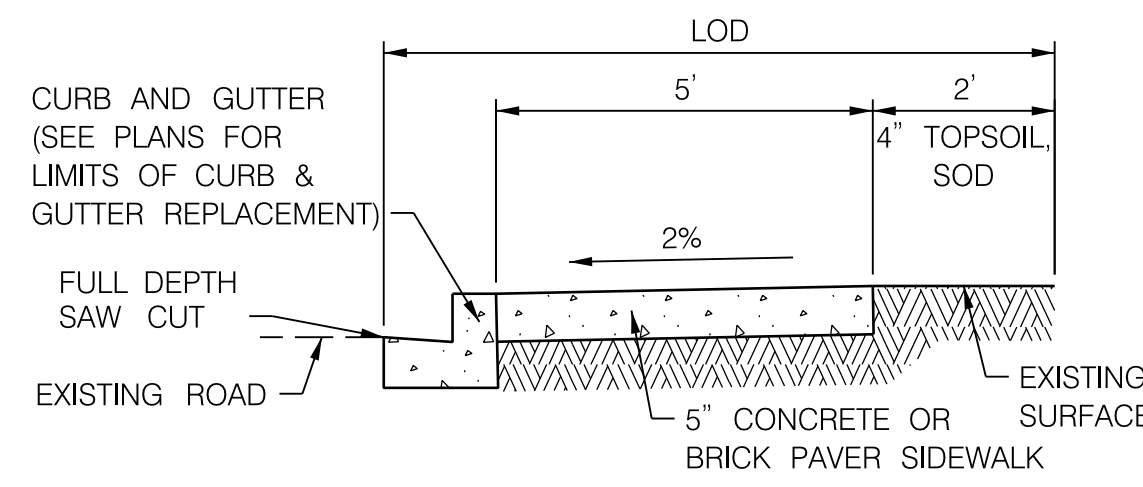
INNOVATIVE CONTRACTING DIVISION

MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

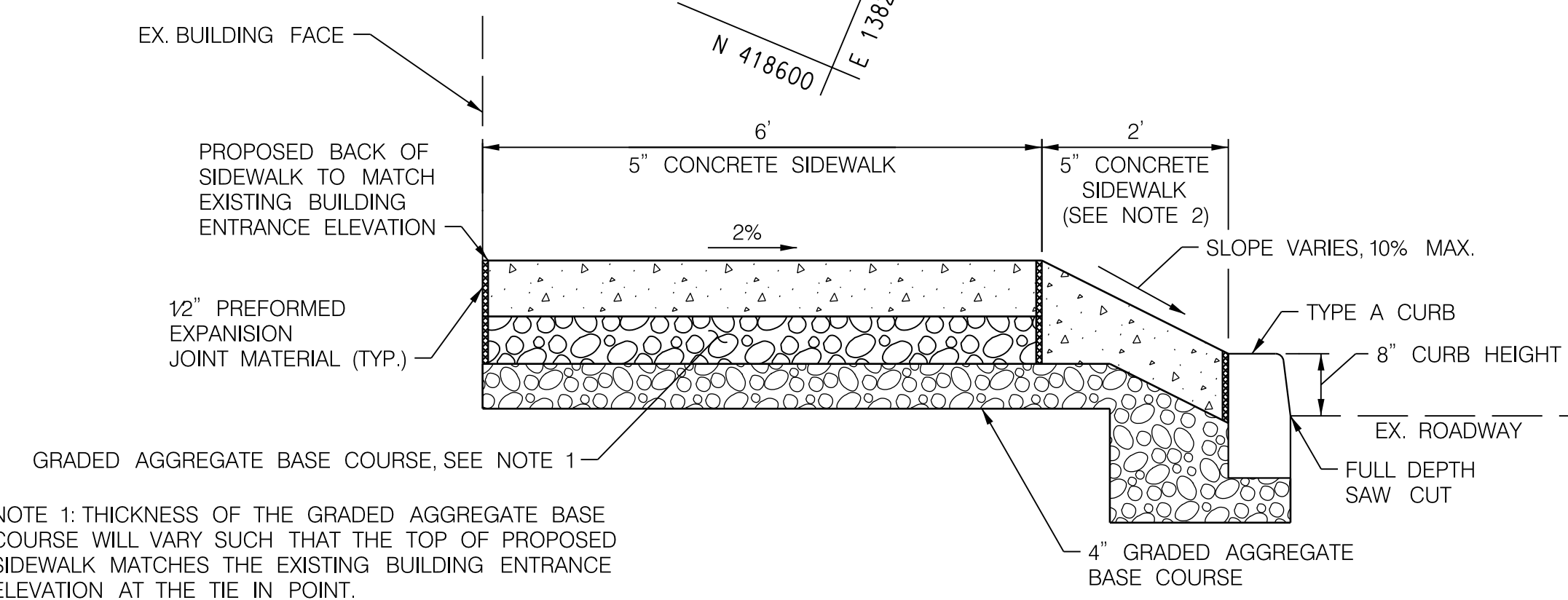
ROADWAY PLAN SHEET			
SCALE	1" = 15'	DATE	APRIL 12, 2022
CONTRACT NO.	PG115133	DESIGNED BY	HWP
COUNTY	PRINCE GEORGE'S	DRAWN BY	SAZ
LOGMILE		CHECKED BY	REH
HORIZONTAL SCALE	N/A	MDE/PRD	20-PR-0006-03
VERTICAL SCALE	N/A	DRAWING NO.	PS-01
SHEET NO.	3 OF 29	OF	11

- LEGEND**
- MILL & OVERLAY
 - CONCRETE SIDEWALK
 - CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
 - 8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
 - NEW CURB
 - APPROX. RIGHT OF WAY & PARCEL LINE
 - APPROX. 100-YEAR FLOODPLAIN



LIMIT OF DISTURBANCE FOR SIDEWALK CONSTRUCTION WITHOUT BUFFER
NOT TO SCALE

- NOTES:**
- DIRECTION OF 2% CROSS SLOPE WILL BE CONSISTENT WITH EXISTING DRAINAGE CONDITIONS.
 - THE LIMIT OF DISTURBANCE WILL BE FROM THE BACK OF CURB TO 2' BEYOND THE EDGE OF SIDEWALK.
 - A CURB SHALL BE ADDED TO THE BACK OF THE SIDEWALK WHERE NEEDED TO PROTECT EXISTING LANDSCAPING AND WHERE THE SLOPE BEHIND THE SIDEWALK IS GREATER THAN 10 PERCENT.



NEW CONCRETE SIDEWALK SECTION
STA. 7+53.50 TO STA. 8+44.00, LT.

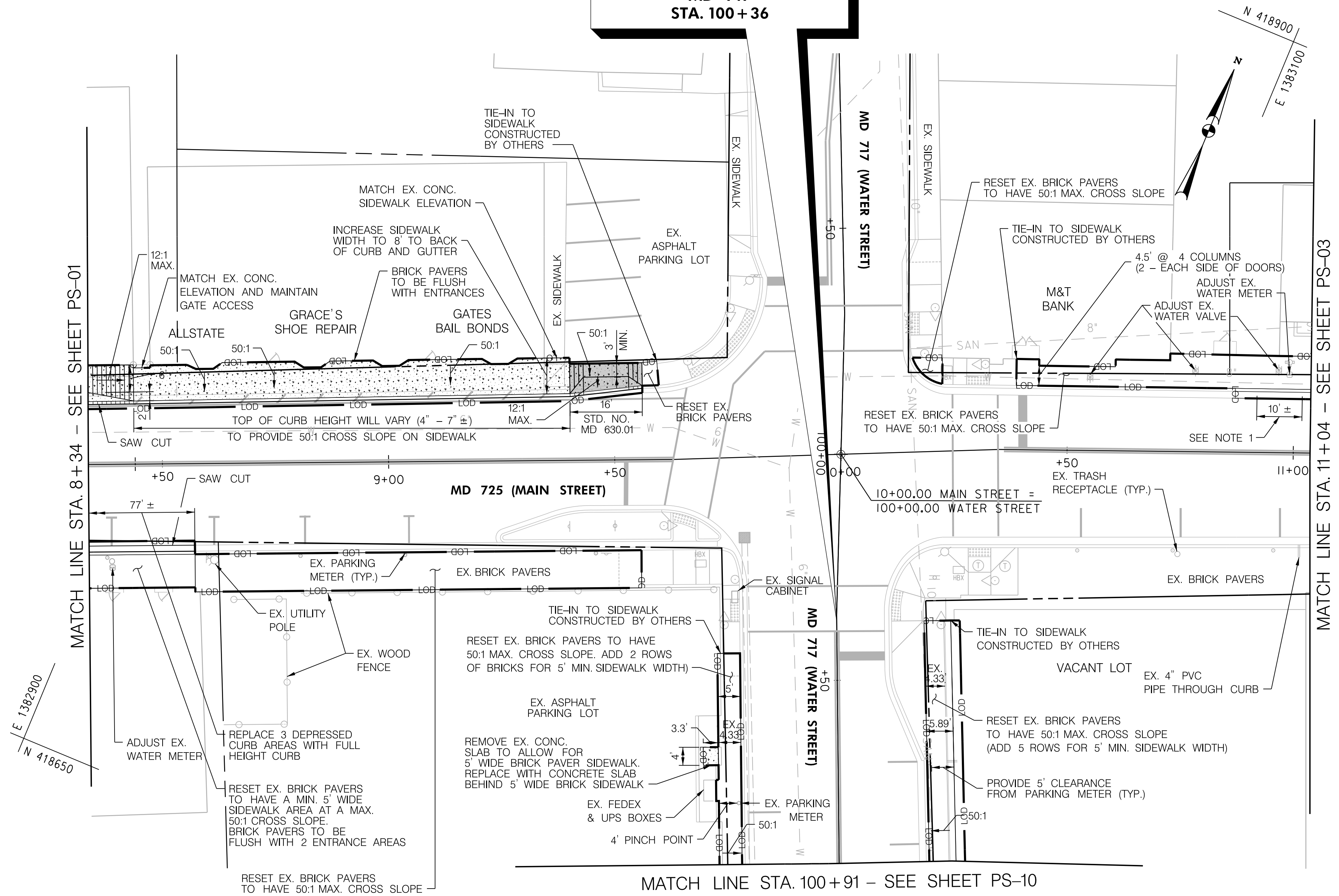
NOTE 1: THICKNESS OF THE GRADED AGGREGATE BASE COURSE WILL VARY SUCH THAT THE TOP OF PROPOSED SIDEWALK MATCHES THE EXISTING BUILDING ENTRANCE ELEVATION AT THE TIE IN POINT. PAID FOR AS 4 INCH GRADED AGGREGATE BASE COURSE.

NOTE 2: PAID FOR AS 5" CONCRETE SIDEWALK.

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: mknowles

LIMIT OF WORK
CONTRACT NO. PGA115133
MD 717
STA. 100+36



ADJUST WATER VALVE BOX			
Q	STA. & OFFSET	EA	REMARKS
MD 725	10+55, L.T.	1	
MD 725	10+78, L.T.	1	
MD 725	10+97, L.T.	1	

ADJUST WATER METER FRAME AND COVER			
Q	STA. & OFFSET	EA	REMARKS
MD 725	8+39, RT.	1	
MD 725	10+99, L.T.	1	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH			
Q	STA. & OFFSET	SY	REMARKS
MD 717	100+36 TO 100+90, L.T.	12	

TURF SOD ESTABLISHMENT			
Q	STA. & OFFSET	SY	REMARKS
MD 717	100+36 TO 100+90, L.T.	12	

BRICK SIDEWALKS			
Q	STA. & OFFSET	SF	REMARKS
MD 717	100+45 TO 100+91, RT.	31	
MD 717	100+36 TO 100+91, L.T.	83	

REMOVE AND RESET BRICK SIDEWALK			
Q	STA. & OFFSET	SF	REMARKS
MD 725	8+34 TO 9+56, RT.	987	
MD 725	9+56 TO 9+60, L.T.	18	
MD 725	10+16 TO 10+23, L.T.	27	
MD 725	10+39 TO 11+04, L.T.	407	
MD 717	100+45 TO 100+91, RT.	205	
MD 717	100+36 TO 100+90, L.T.	276	

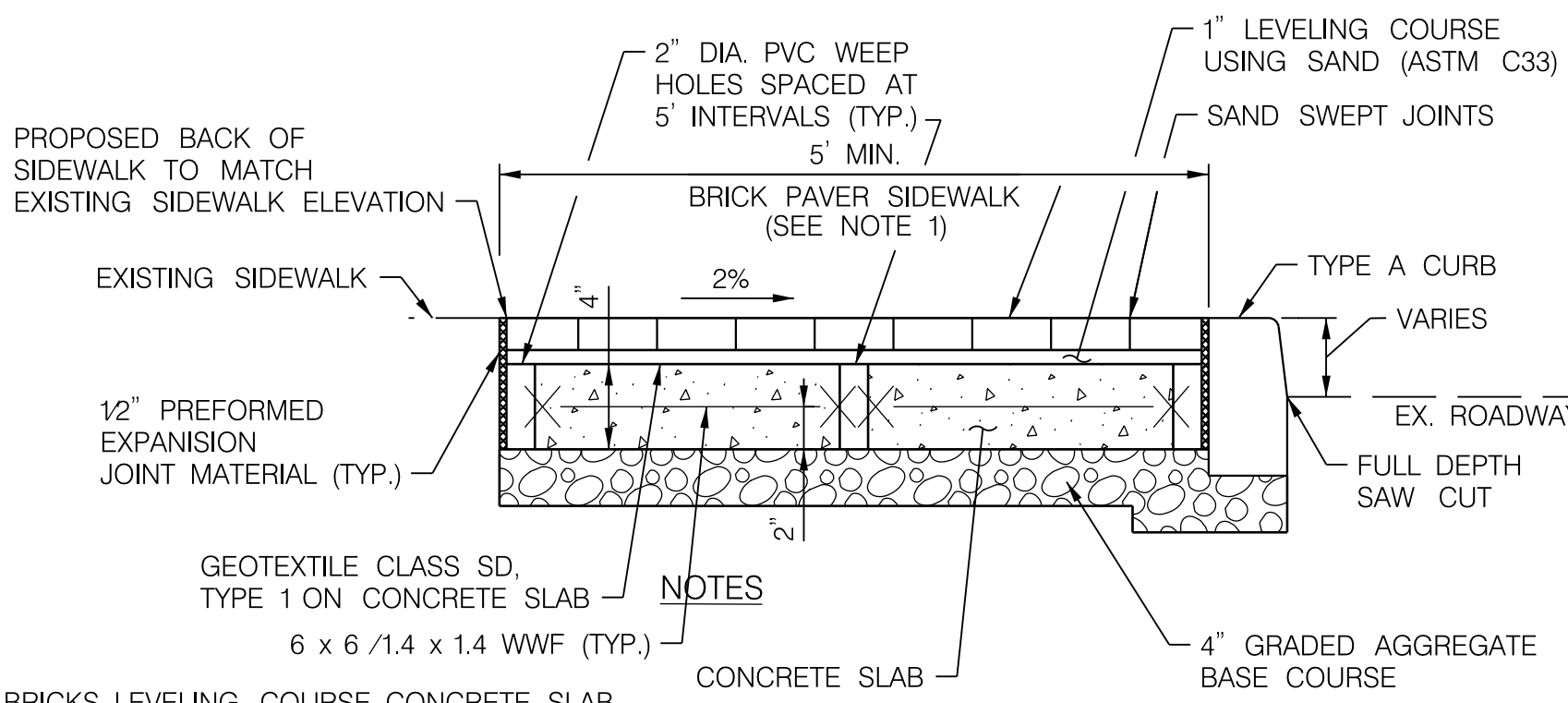
5 INCH CONCRETE SIDEWALK			
Q	STA. & OFFSET	SF	REMARKS
MD 725	8+34 TO 9+40, L.T.	822	
MD 717	100+65 TO 100+69, RT.	13	

TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH			
Q	STA. & OFFSET	LF	REMARKS
MD 725	8+34 TO 8+57, RT.	23	
MD 725	10+87 TO 11+04, L.T.	17	

TYPE A CURB ANY HEIGHT OR DEPTH			
Q	STA. & OFFSET	LF	REMARKS
MD 725	8+34 TO 9+56, L.T.	123	

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9			
Q	STA. & OFFSET	SY	REMARKS
MD 725	9+40 TO 9+56, L.T.	10	STD. NO. MD 630.01

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.



NOTE 1: BRICKS, LEVELING COURSE, CONCRETE SLAB, EXPANSION JOINT MATERIALS, GEOTEXTILES, & WWF ARE INCIDENTAL TO THE BRICK SIDEWALK CONSTRUCTION.

NEW BRICK SIDEWALK SECTION

- LEGEND**
- MILL & OVERLAY
 - CONCRETE SIDEWALK
 - CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
 - 8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
 - NEW CURB
 - APPROX. RIGHT OF WAY & PARCEL LINE
 - APPROX. 100-YEAR FLOODPLAIN

- REMOVE EX. DEPRESSED CURB. REPLACE WITH VARIABLE HEIGHT CURB & GUTTER TO ENSURE THE PROPOSED SIDEWALK WILL TIE IN AT EX. CONCRETE OR BRICK SIDEWALK.



INNOVATIVE CONTRACTING DIVISION

STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

ROADWAY PLAN SHEET

SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PGA115133

DESIGNED BY HWP COUNTY PRINCE GEORGE'S
DRAWN BY SAZ LOGMILE
CHECKED BY REH HORIZONTAL SCALE N/A
MDE/PRD 20-PR-0006-03 VERTICAL SCALE N/A

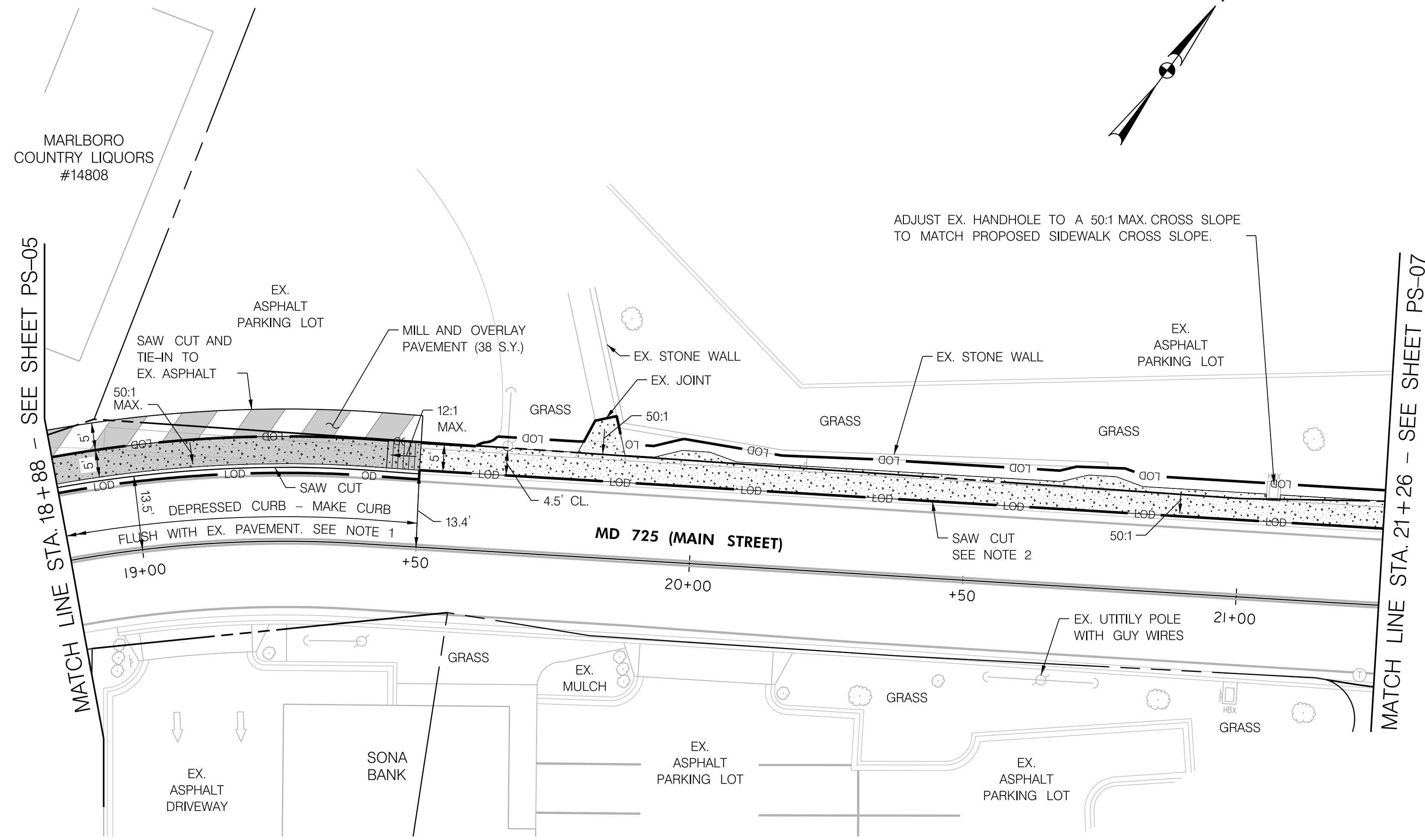
DRAWING NO. **PS-02** OF **11** SHEET NO. 4 OF 29

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
FINAL APPROVAL
3/3/2022
Chief, Plan Review Division
PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
MODIFICATION 1 APPROVAL
3/3/2022
Chief, Plan Review Division
PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

E 1383700
N 419300

E 1383700
N 419200



TURF SOD ESTABLISHMENT			
Q	STA. & OFFSET	SY	REMARKS
MD 725	19+60 TO 19+79, LT.	3	
MD 725	19+87 TO 21+26, LT.	31	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH			
Q	STA. & OFFSET	SY	REMARKS
MD 725	19+60 TO 19+79, LT.	3	
MD 725	19+87 TO 21+26, LT.	31	

ADJUST EXISTING HANDHOLE			
Q	STA. & OFFSET	EA	REMARKS
MD 725	21+06, LT.	1	

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9			
Q	STA. & OFFSET	SY	REMARKS
MD 725	18+88 TO 19+50, LT.	38	

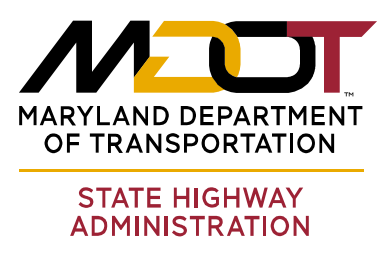
5 INCH CONCRETE SIDEWALK			
Q	STA. & OFFSET	SF	REMARKS
MD 725	19+50 TO 21+26, LT.	949	

TYPE A CURB ANY HEIGHT OR DEPTH			
Q	STA. & OFFSET	LF	REMARKS
MD 725	18+88 TO 19+50, LT.	66	

STANDARD MILLING ASPHALT PAVEMENT OVER 1 INCH TO 2.5 INCH DEPTH			
Q	STA. & OFFSET	SY	REMARKS
MD 725	18+88 TO 19+50, LT.	38	

SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE PG 64E-22, LEVEL 2			
Q	STA. & OFFSET	TON	REMARKS
MD 725	18+88 TO 19+50, LT.	4	

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.



INNOVATIVE CONTRACTING DIVISION
MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

ROADWAY PLAN SHEET

SCALE 1" = 15'	DATE APRIL 12, 2022	CONTRACT NO. PGAT15133
DESIGNED BY HWP	COUNTY PRINCE GEORGE'S	
DRAWN BY SAZ	LOGMILE	
CHECKED BY REH	HORIZONTAL SCALE N/A	
MDE/PRD 20-PR-0006-03	VERTICAL SCALE N/A	
DRAWING NO. PS-06	OF 11	SHEET NO. 8 OF 29

NOTES

- CONTRACTOR TO INSTALL CURB FLUSH WITH THE EXISTING ROADWAY AND SIDEWALK AT A 50:1 MAX. CROSS SLOPE TO ENSURE POSITIVE DRAINAGE.
- FROM STA. 19+50, LT. TO STA. 22+64, LT., CONTRACTOR SHALL SAW CUT ALONG THE BACK OF EX. CURB. REMOVE EX. CONCRETE SIDEWALK AND PRESERVE THE EX. CURB. INSTALL NEW CONCRETE SIDEWALK AT 50:1 MAX CROSS SLOPE USING THE EX. CURB UNLESS OTHERWISE NOTED ON THE PLANS.

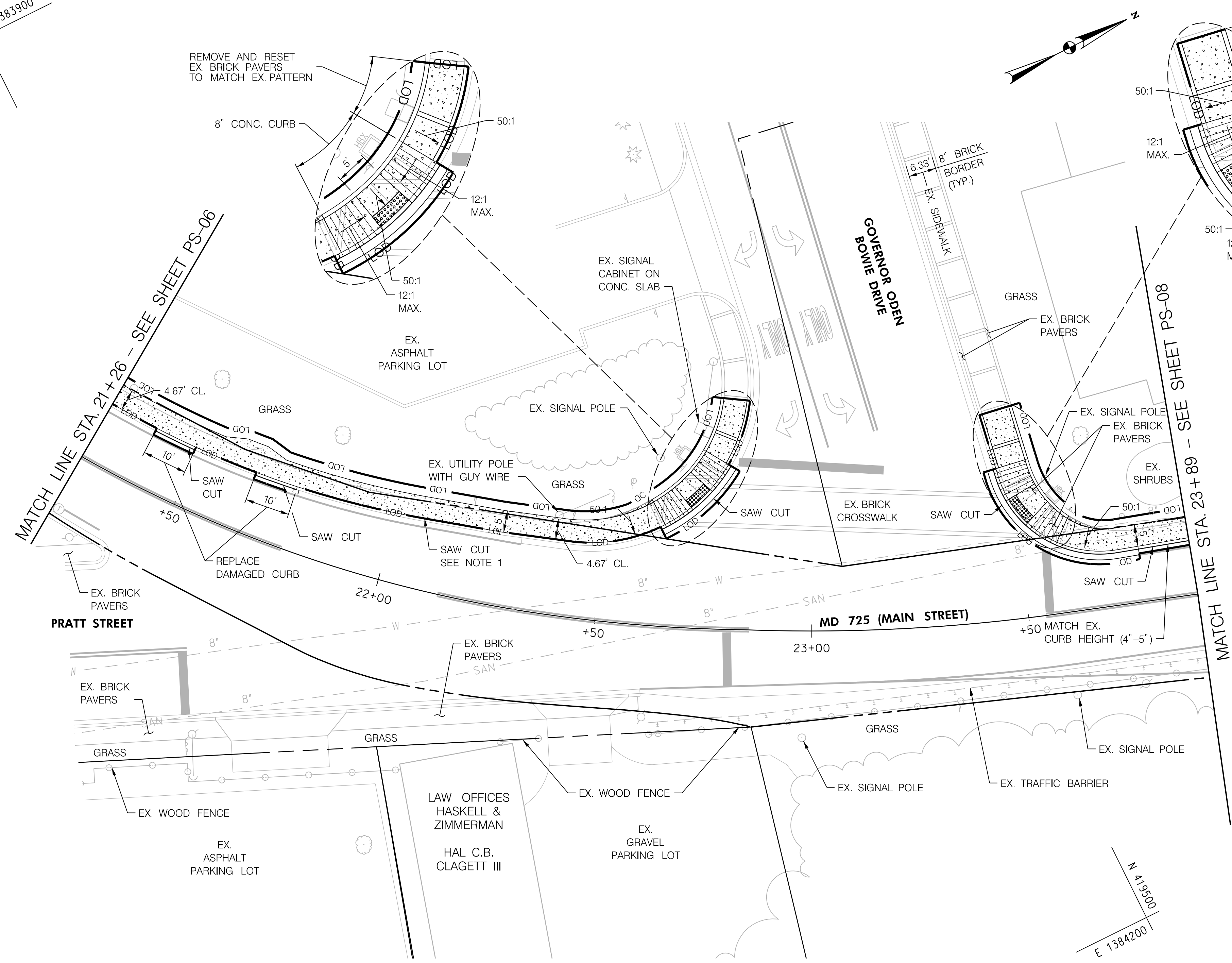
LEGEND

	MILL & OVERLAY
	CONCRETE SIDEWALK
	CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
	8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
	NEW CURB
	APPROX. RIGHT OF WAY & PARCEL LINE
	APPROX. 100-YEAR FLOODPLAIN



BY: mknowles

E 1383900
N 419350



REMOVE AND RESET
EX. BRICK PAVERS
TO MATCH EX. PATTERN

8" CONC. CURB

REMOVAL OF EXISTING SIDEWALK			
STATION	STA. & OFFSET	CY	REMARKS
MD 725	22+50 TO 22+61, LT.	0.1	EX. RAMP & SW REMOVAL
MD 725	23+62 TO 23+69, LT.	0.1	EX. RAMP & SW REMOVAL

REMOVAL AND RESET BRICK SIDEWALK			
STATION	STA. & OFFSET	SF	REMARKS
MD 725	22+73 TO 22+85, LT.	23	BRICK BORDER
MD 725	23+44 TO 23+55, LT.	21	BRICK BORDER

5 INCH CONCRETE SIDEWALK			
STATION	STA. & OFFSET	SF	REMARKS
MD 725	21+26 TO 22+84, LT.	861	
MD 725	23+44 TO 23+89, LT.	362	

TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH			
STATION	STA. & OFFSET	LF	REMARKS
MD 725	22+64 TO 22+82, LT.	23	
MD 725	23+44 TO 23+77, LT.	45	

TYPE A CURB ANY HEIGHT OR DEPTH			
STATION	STA. & OFFSET	LF	REMARKS
MD 725	21+38 TO 21+49, LT.	10	
MD 725	21+65 TO 21+75, LT.	10	
MD 725	22+61 TO 22+74, LT.	17	
MD 725	23+54 TO 23+62, LT.	17	
MD 725	23+77 TO 23+89, LT.	12	

DETECTABLE WARNING SURFACES STD. NO. MD 655.40			
STATION	STATION	SF	REMARKS
MD 725	22+73, LT.	12	COLOR TO BE RED
MD 725	23+50, LT.	14	COLOR TO BE RED

TURF SOD ESTABLISHMENT			
STATION	STA. & OFFSET	SY	REMARKS
MD 725	21+26 TO 22+84, LT.	35	
MD 725	23+52 TO 23+89, LT.	11.5	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH			
STATION	STA. & OFFSET	SY	REMARKS
MD 725	21+26 TO 22+84, LT.	35	
MD 725	23+52 TO 23+89, LT.	11.5	

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.

SIDEWALK RAMPS		
STATION	STA. & OFFSET	REMARKS
MD 725	22+73, LT.	STD. NO. MD 655.12
MD 725	23+50, LT.	STD. NO. MD 655.12

NOTES
1. FROM STA. 19+50, LT. TO STA. 22+64, LT., CONTRACTOR SHALL SAW CUT ALONG THE BACK OF EX. CURB. REMOVE EX. CONCRETE SIDEWALK AND PRESERVE THE EX. CURB. INSTALL NEW CONCRETE SIDEWALK AT 50:1 MAX CROSS SLOPE USING THE EX. CURB UNLESS OTHERWISE NOTED ON THE PLANS.

LEGEND

	MILL & OVERLAY
	CONCRETE SIDEWALK
	CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
	8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
	NEW CURB
	APPROX. RIGHT OF WAY & PARCEL LINE
	APPROX. 100-YEAR FLOODPLAIN

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

INNOVATIVE CONTRACTING DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

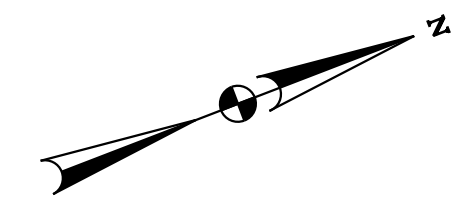
ROADWAY PLAN SHEET

SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PGAT15133

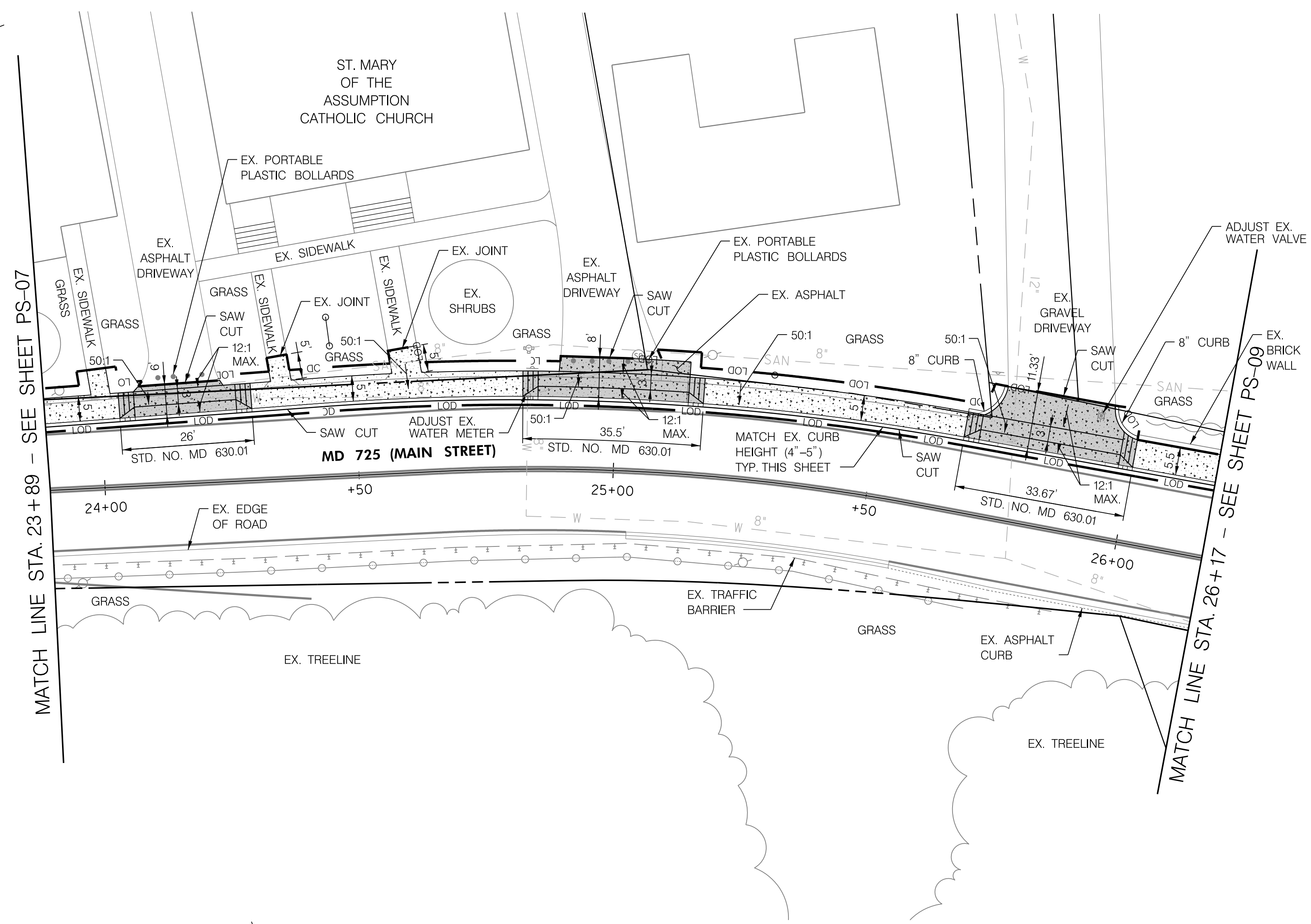
DESIGNED BY HWP COUNTY PRINCE GEORGE'S
DRAWN BY SAZ LOGMILE
CHECKED BY REH HORIZONTAL SCALE N/A
MDE/PRD 20-PR-0006-03 VERTICAL SCALE N/A

DRAWING NO. **PS-07** OF 11 SHEET NO. 9 OF 29

E 1384050
N 413100



E 1384050
N 4139550



ADJUST WATER METER FRAME AND COVER

Q	STA. & OFFSET	EA	REMARKS
MD 725	24+83, LT.	1	

ADJUST WATER VALVE BOX

Q	STA. & OFFSET	EA	REMARKS
MD 725	25+93, LT.	1	

5 INCH CONCRETE SIDEWALK

Q	STA. & OFFSET	SF	REMARKS
MD 725	23+89 TO 24+04, LT.	92	
MD 725	24+29 TO 24+82, LT.	316	
MD 725	25+17 TO 25+67, LT.	260	
MD 725	26+00 TO 26+17, LT.	91	

TYPE A CURB ANY HEIGHT OR DEPTH

Q	STA. & OFFSET	LF	REMARKS
MD 725	23+89 TO 26+17, LT.	231	
MD 725	25+67 TO 25+74, LT.	11	
MD 725	25+95 TO 26+00, LT.	9	

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9

Q	STA. & OFFSET	SY	REMARKS
MD 725	24+04 TO 24+29, LT.	16	STD. NO. MD 630.01
MD 725	24+82 TO 25+17, LT.	28	STD. NO. MD 630.01
MD 725	25+67 TO 26+00, LT.	36	STD. NO. MD 630.01

TURF SOD ESTABLISHMENT

Q	STA. & OFFSET	SY	REMARKS
MD 725	23+89 TO 23+98, LT.	3	
MD 725	24+01 TO 24+08, LT.	2	
MD 725	24+23 TO 24+33, LT.	2.5	
MD 725	24+37 TO 24+57, LT.	4.5	
MD 725	24+63 TO 24+89, LT.	6.5	
MD 725	25+07 TO 25+73, LT.	16	
MD 725	25+96 TO 26+00, LT.	1.5	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH

Q	STA. & OFFSET	SY	REMARKS
MD 725	23+89 TO 23+98, LT.	3	
MD 725	24+01 TO 24+08, LT.	2	
MD 725	24+23 TO 24+33, LT.	2.5	
MD 725	24+37 TO 24+57, LT.	4.5	
MD 725	24+63 TO 24+89, LT.	6.5	
MD 725	25+07 TO 25+73, LT.	16	
MD 725	25+96 TO 26+00, LT.	1.5	

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.

INNOVATIVE CONTRACTING DIVISION

STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

ROADWAY PLAN SHEET

SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PG151533

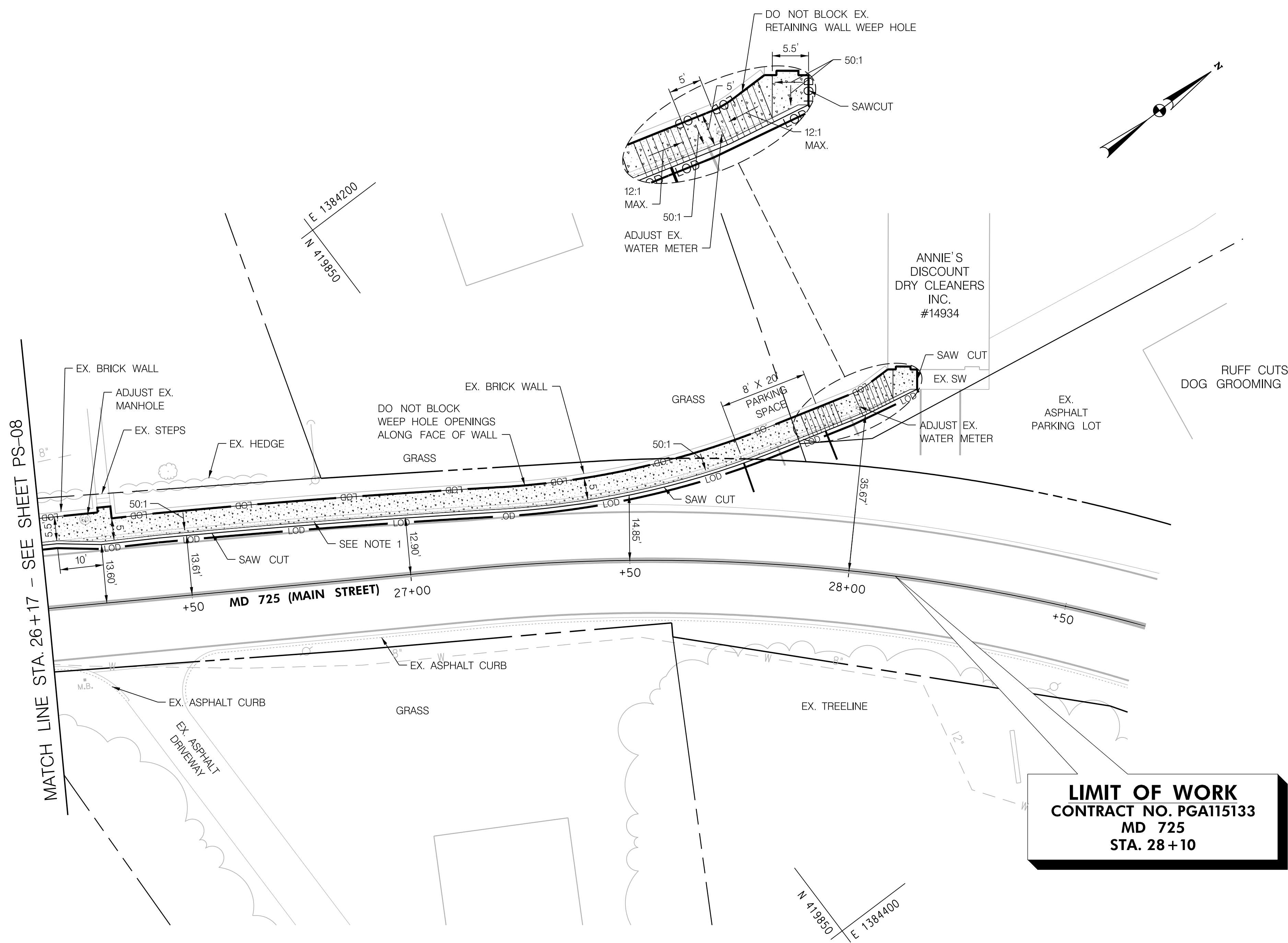
DESIGNED BY HWP COUNTY PRINCE GEORGE'S
DRAWN BY SAZ LOGMILE
CHECKED BY REH HORIZONTAL SCALE N/A
MDE/PRD 20-PR-0006-03 VERTICAL SCALE N/A

DRAWING NO. **PS-08** OF **11** SHEET NO. 10 OF 29

- LEGEND**
- MILL & OVERLAY
 - CONCRETE SIDEWALK
 - CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
 - 8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
 - NEW CURB
 - APPROX. RIGHT OF WAY & PARCEL LINE
 - APPROX. 100-YEAR FLOODPLAIN

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: mknowles



LIMIT OF WORK
CONTRACT NO. PG115133
MD 725
STA. 28+10

5 INCH CONCRETE SIDEWALK			
Q	STA. & OFFSET	SF	REMARKS
MD 725	26+17 TO 28+10, LT.	1051	

TYPE A CURB ANY HEIGHT OR DEPTH			
Q	STA. & OFFSET	LF	REMARKS
MD 725	26+17 TO 28+10, LT.	204	
*STA. 26+33 TO 27+88, LT. VARY THE CURB HEIGHT TO ENSURE THE PROPOSED SIDEWALK DOES NOT BLOCK THE RETAINING WALL WEEP HOLES.			

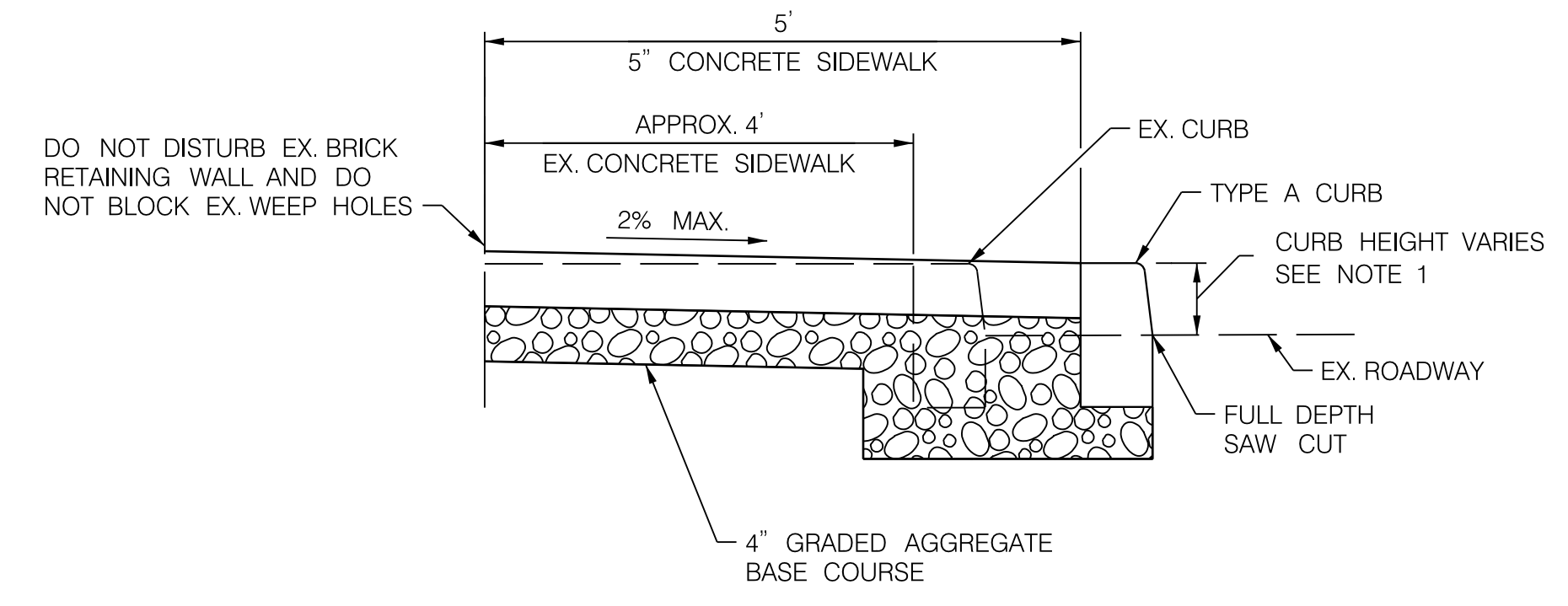
SIDEWALK RAMPS			
Q	STA. & OFFSET	REMARKS	
MD 725	27+97, LT.	STD. NO. MD 655.12 (MOD. - NO DWS)	

ADJUST WATER METER FRAME AND COVER			
Q	STA. & OFFSET	EA	REMARKS
MD 725	27+98, LT.	1	

ADJUST SEWER MANHOLE FRAME AND COVER (24 INCH)			
Q	STA. & OFFSET	EA	REMARKS
MD 725	26+27, LT.	1	STORM DRAIN

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.

- LEGEND**
- MILL & OVERLAY
 - CONCRETE SIDEWALK
 - CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
 - 8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
 - NEW CURB
 - APPROX. RIGHT OF WAY & PARCEL LINE
 - APPROX. 100-YEAR FLOODPLAIN



NEW CONCRETE SIDEWALK WIDENING SECTION
STA. 26+32, LT. TO STA. 27+89, LT.
NOT TO SCALE

- NOTES**
- FROM STA. 26+33, LT. TO STA. 27+89, LT., CONTRACTOR SHALL VARY THE CURB HEIGHT SO THAT THE PROPOSED SIDEWALK DOES NOT BLOCK THE WEEP HOLES ON THE EX. RETAINING WALL.

INNOVATIVE CONTRACTING DIVISION

STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

ROADWAY PLAN SHEET			
SCALE 1" = 15'	DATE APRIL 12, 2022	CONTRACT NO. PG115133	
DESIGNED BY HWP	COUNTY PRINCE GEORGE'S		
DRAWN BY SAZ	LOGMILE		
CHECKED BY REH	HORIZONTAL SCALE N/A		
MDE/PRD 20-PR-0006-03	VERTICAL SCALE N/A		
DRAWING NO. PS-09	OF 11	SHEET NO. 11	OF 29

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
FINAL APPROVAL
3/3/2022
Chief, Plan Review Division
PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
MODIFICATION 1 APPROVAL
3/3/2022
Chief, Plan Review Division
PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

SHREDDED HARDWOOD BARK MULCHING
3 INCH DEPTH

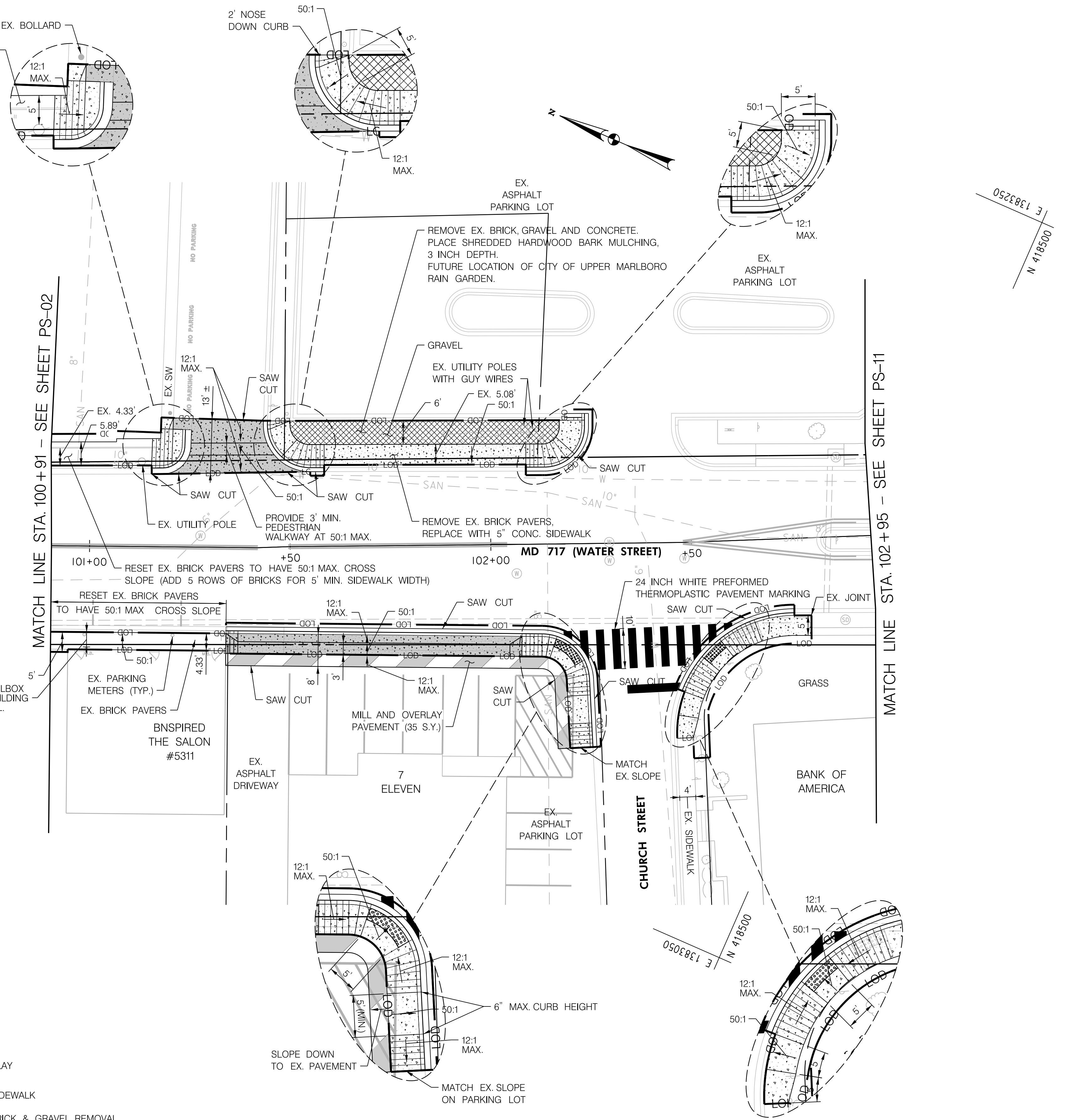
Q	STA. & OFFSET	SY	REMARKS
MD 717	101+50 TO 102+17, LT.	44	

TURF SOD ESTABLISHMENT

Q	STA. & OFFSET	SY	REMARKS
MD 717	100+91 TO 101+17, LT.	6	
MD 717	102+50 TO 102+79, RT.	9.5	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH

Q	STA. & OFFSET	SY	REMARKS
MD 717	100+91 TO 101+17, LT.	6	
MD 717	102+50 TO 102+79, RT.	9.5	



E 1383050
N 418750

E 1383250
N 418500

- LEGEND**
- MILL & OVERLAY
 - CONCRETE SIDEWALK
 - CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
 - 8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
 - NEW CURB
 - APPROX. RIGHT OF WAY & PARCEL LINE
 - APPROX. 100-YEAR FLOODPLAIN

DETECTABLE WARNING SURFACES
STD. NO. MD 655.40

Q	STATION	SF	REMARKS
MD 717	102+20, RT.	16	COLOR TO BE RED
MD 717	102+55, RT.	12	COLOR TO BE RED

REMOVAL OF EXISTING SIDEWALK

Q	STA. & OFFSET	CY	REMARKS
MD 717	101+50 TO 102+17, LT.	6	EX. RAMP & SW REMOVAL

REMOVE AND RESET BRICK SIDEWALK

Q	STA. & OFFSET	SF	REMARKS
MD 717	100+91 TO 101+15, LT.	134	
MD 717	100+91 TO 101+34, RT.	186	

BRICK SIDEWALKS

Q	STA. & OFFSET	SF	REMARKS
MD 717	100+91 TO 101+15, LT.	114	
MD 717	100+91 TO 100+95, RT.	3	

5 INCH CONCRETE SIDEWALK

Q	STA. & OFFSET	SF	REMARKS
MD 717	101+15 TO 101+23, LT.	66	
MD 717	101+45 TO 102+23, LT.	427	
MD 717	102+08 TO 102+24, RT.	176	
MD 717	102+46 TO 102+80, RT.	235	

TYPE A COMBINATION CURB AND GUTTER
ANY HEIGHT OR DEPTH

Q	STA. & OFFSET	LF	REMARKS
MD 717	101+17 TO 101+25, LT.	16	
MD 717	101+34 TO 102+26, RT.	115	
MD 717	101+43 TO 101+58, LT.	23	
MD 717	102+09 TO 102+19, LT.	29	
MD 717	102+46 TO 102+75, RT.	37	

STANDARD MILLING ASPHALT PAVEMENT
OVER 1 INCH TO 2.5 INCH DEPTH

Q	STA. & OFFSET	SY	REMARKS
MD 725	101+34 TO 102+19, RT.	35	

SUPERPAVE ASPHALT MIX 9.5MM FOR
SURFACE PG 64E-22, LEVEL 2

Q	STA. & OFFSET	TON	REMARKS
MD 725	101+34 TO 102+19, RT.	4	

8 INCH PORTLAND CEMENT CONCRETE
PAVEMENT FOR DRIVEWAY MIX 9

Q	STA. & OFFSET	SY	REMARKS
MD 717	101+18 TO 101+55, LT.	33	STD. NO. MD 630.01
MD 717	101+34 TO 102+08, RT.	41	STD. NO. MD 630.01

SIDEWALK RAMPS

Q	STA. & OFFSET	REMARKS
MD 717	101+23, LT.	STD. NO. MD 655.11 (MOD. - NO DWS)
MD 717	101+46, LT.	STD. NO. MD 655.12 (MOD. - NO DWS)
MD 717	102+20, RT.	STD. NO. MD 655.12
MD 717	102+23, LT.	STD. NO. MD 655.12 (MOD. - NO DWS)
MD 717	102+54, RT.	STD. NO. MD 655.12

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.

INNOVATIVE CONTRACTING DIVISION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

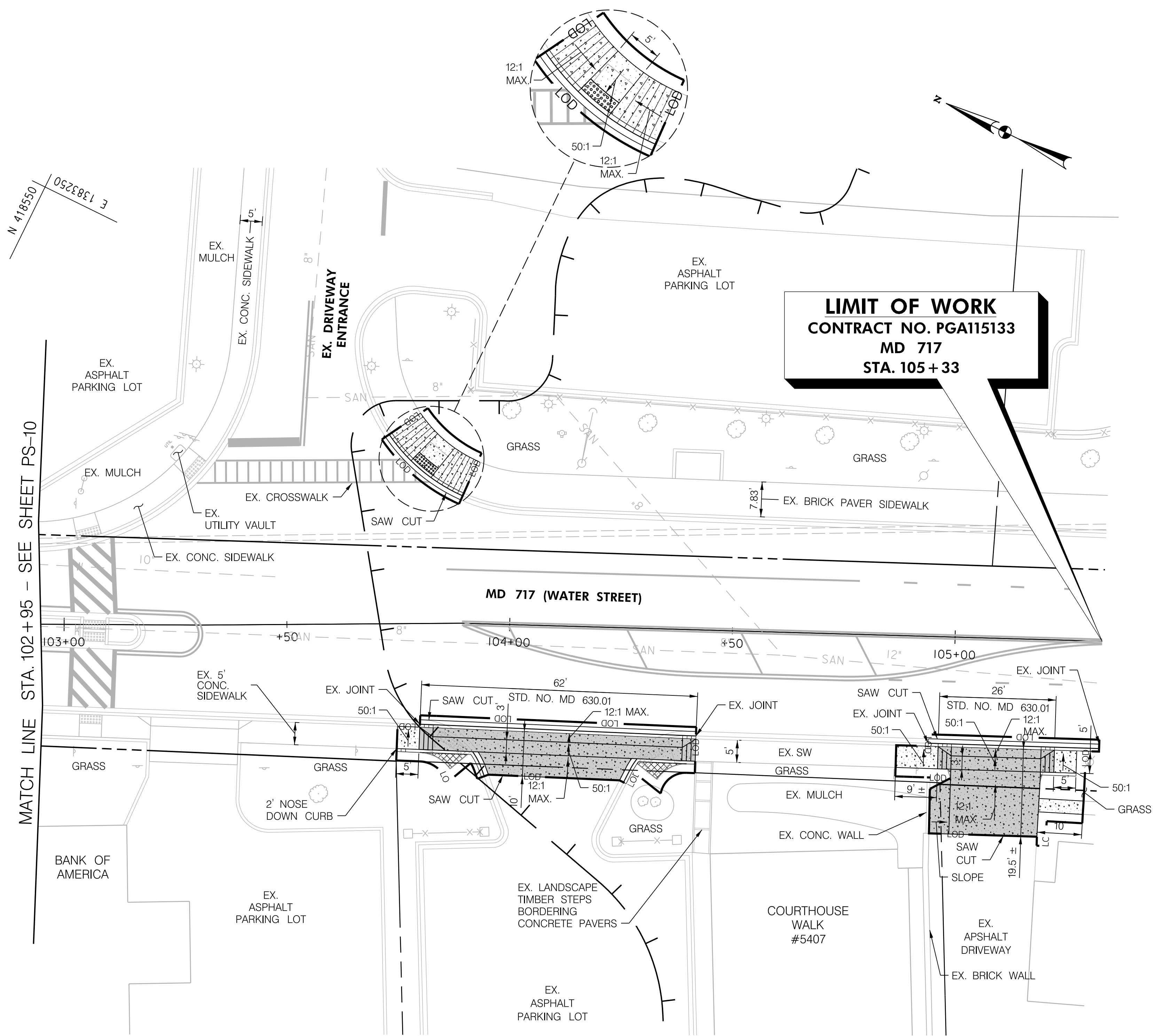
ROADWAY PLAN SHEET

SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PG151533

DESIGNED BY HWP COUNTY PRINCE GEORGE'S
DRAWN BY SAZ LOGMILE
CHECKED BY REH HORIZONTAL SCALE N/A
MDE/PRD 20-PR-0006-03 VERTICAL SCALE N/A

DRAWING NO. **PS-10** OF **11** SHEET NO. 12 OF 29

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231



TURF SOD ESTABLISHMENT

STATION	STA. & OFFSET	SY	REMARKS
MD 717	103+80 TO 103+94, LT.	4	
MD 717	103+74 TO 103+93, RT.	5	
MD 717	104+27 TO 104+42, RT.	5	
MD 717	104+87 TO 105+00, RT.	3	
MD 717	105+20 TO 105+30, RT.	3.5	
MD 717	105+20 TO 105+30, RT.	7.5	

REMOVAL OF EXISTING SIDEWALK

STATION	STA. & OFFSET	CY	REMARKS
MD 717	103+80 TO 103+90, RT.	0.25	EX. RAMP & SW REMOVAL
MD 717	104+29 TO 104+40, RT.	0.25	EX. RAMP & SW REMOVAL
MD 717	104+99 TO 105+21, RT.	0.1	EX. RAMP & SW REMOVAL

5 INCH CONCRETE SIDEWALK

STATION	STA. & OFFSET	SF	REMARKS
MD 717	103+74 TO 103+80, RT.	25	
MD 717	103+74 TO 103+93, LT.	150	
MD 717	104+87 TO 104+97, RT.	47	
MD 717	105+20 TO 105+30, RT.	30	
MD 717	105+23 TO 105+28, RT.	25	

TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH

STATION	STA. & OFFSET	LF	REMARKS
MD 717	103+72 TO 103+90, LT.	22	
MD 717	103+80 TO 104+42, RT.	62	
MD 717	103+92 TO 103+96, RT.	6	
MD 717	104+26 TO 104+30, RT.	6	
MD 717	104+95 TO 105+33, RT.	38	

TYPE A CURB ANY HEIGHT OR DEPTH

STATION	STA. & OFFSET	LF	REMARKS
MD 717	103+74 TO 103+91, RT.	16	
MD 717	104+30 TO 104+42, RT.	13	
MD 717	105+20 TO 105+28, RT.	8	

SIDEWALK RAMPS

STATION	STA. & OFFSET	REMARKS
MD 717	103+81, LT.	STD. NO. MD 655.12

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9

STATION	STA. & OFFSET	SY	REMARKS
MD 717	103+80 TO 104+42, RT.	52	STD. NO. MD 630.01
MD 717	104+97 TO 105+23, RT.	52	STD. NO. MD 630.01

DETECTABLE WARNING SURFACES STD. NO. MD 655.40

STATION	STATION	SF	REMARKS
MD 717	103+81, LT.	12	COLOR TO BE RED

NOTE: THE WORK IN THIS AREA SHALL BE DONE USING THE METHOD OF "SAME DAY STABILIZATION." NO MORE AREA SHALL BE DISTURBED THAN CAN BE STABILIZED BY THE END OF THE WORK DAY. ALL AREAS SHALL BE STABILIZED USING TEMPORARY MULCHING OR GRADED AGGREGATE BASE COURSE BY THE END OF THE WORKDAY AS DIRECTED BY THE ENGINEER.

PLACING FURNISHED TOPSOIL 4 INCH DEPTH

STATION	STA. & OFFSET	SY	REMARKS
MD 717	103+80 TO 103+94, LT.	4	
MD 717	103+74 TO 103+93, RT.	5	
MD 717	104+27 TO 104+42, RT.	5	
MD 717	104+87 TO 105+00, RT.	3	
MD 717	105+20 TO 105+30, RT.	3.5	
MD 717	105+20 TO 105+30, RT.	7.5	

- LEGEND**
- MILL & OVERLAY
 - CONCRETE SIDEWALK
 - CONCRETE, BRICK & GRAVEL REMOVAL (PAID FOR AS CLASS 2 EXCAVATION)
 - 8 INCH PORTLAND CEMENT CONCRETE DRIVEWAY
 - NEW CURB
 - APPROX. RIGHT OF WAY & PARCEL LINE
 - APPROX. 100-YEAR FLOODPLAIN

INNOVATIVE CONTRACTING DIVISION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

ROADWAY PLAN SHEET

SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PGA115133

DESIGNED BY HWP COUNTY PRINCE GEORGE'S
DRAWN BY SAZ LOGMILE
CHECKED BY REH HORIZONTAL SCALE N/A
MDE/PRD 20-PR-0006-03 VERTICAL SCALE N/A

DRAWING NO. **PS-11** OF 11 SHEET NO. 13 OF 29

EROSION AND SEDIMENT CONTROL – GENERAL NOTES

1. NOTIFICATION

NOTIFY THE REGIONAL ENVIRONMENTAL COORDINATOR (REC) IN WRITING AND/OR BY TELEPHONE AT (410) 365-0164 PRIOR TO THE FOLLOWING POINTS:

- PRE-CONSTRUCTION MEETING.
- EROSION AND SEDIMENT CONTROL (ESC) MEETING (MINIMUM 7 WORKING DAYS PRIOR TO COMMENCING EARTH DISTURBING ACTIVITIES).
- UPON INSTALLATION OF INITIAL ESC MEASURES.
- INSTALLATION OF MAJOR ESC BASINS/TRAPS.
- REMOVAL OR MODIFICATION OF ANY ESC MEASURES.
- REMOVAL OF ALL ESC DEVICES.
- FINAL ACCEPTANCE BY THE ADMINISTRATION.

2. STANDARDS AND SPECIFICATIONS

CONSTRUCT THIS PLAN ACCORDING TO THE MARYLAND DEPARTMENT OF THE ENVIRONMENT (MDE) "2011 MARYLAND STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL", THE MDE "2000 MARYLAND STORMWATER DESIGN MANUAL, VOLUMES I & II", THE MDT SHA "FIELD GUIDE FOR EROSION AND SEDIMENT CONTROL", THE ANNOTATED CODE OF MARYLAND, THE CODE OF MARYLAND (COMAR) 26.17.01 AND 26.17.02, ALL REVISIONS THERE OF, AND AS SPECIFIED. KEEP A COPY OF THE 2011 "MARYLAND STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" ON THE SITE AT ALL TIMES. PERFORM VEGETATIVE STABILIZATION ACCORDING TO THOSE STANDARDS AND AS SPECIFIED.

3. INSPECTION

DAILY INSPECT ALL ESC MEASURES AND MAINTAIN THEM IN A CONTINUOUSLY-EFFECTIVE OPERATING CONDITION UNTIL REMOVED AS APPROVED BY THE REC AND THE ENGINEER.

4. SHUTDOWNS / LIQUIDATED DAMAGES

COMPLETE COMPLIANCE WITH THE APPROVED ESC PLAN IS EXPECTED AT ALL TIMES. IN CASES WHERE THE CONTRACTOR IS FOUND TO BE IN NON-COMPLIANCE, THE ADMINISTRATION WILL TAKE STEPS TO IMPOSE SELECTED OR TOTAL SHUTDOWNS AND MAY IMPOSE LIQUIDATED DAMAGES FOR NON-COMPLIANCE.

THE ADMINISTRATION'S DISTRICT ENGINEER MAY IMPOSE A TOTAL OR PARTIAL SHUTDOWN IF THE PROJECT MAY ADVERSELY IMPACT THE WATERS OF THE STATE.

5. RECORD KEEPING

ENSURE THE STORMWATER MANAGEMENT (SWM)/ESC APPROVAL LETTER, APPROVED EROSION AND SEDIMENT CONTROL PLANS, APPROVED MODIFICATIONS, MODIFICATION APPROVAL LETTER(S), DAILY LOG BOOKS, TEST REPORTS, AND NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) NOTICE OF INTENT (NOI) PERMIT ARE AVAILABLE ON-SITE FOR REVIEW AND INSPECTION BY THE ADMINISTRATION.

6. CLEARING AND GRUBBING

UNLESS OTHERWISE SPECIFIED OR APPROVED, LIMIT THE CLEARING AND GRUBBING AREA TO A SINGLE 20-ACRE GRADING UNIT PER GRADING OPERATION. ONCE THIS FIRST UNIT IS HALF GRADED, STABILIZATION MEASURES ARE IN PLACE, AND APPROVED, WORK MAY PROCEED TO A SECOND 20-ACRE GRADING UNIT. UNLESS SPECIFICALLY APPROVED, NO MORE THAN 30 ACRES MAY BE DISTURBED AT ANY TIME.

7. SENSITIVE AREAS

WITH THE APPROVAL AND ASSISTANCE OF THE ENGINEER, COORDINATE WITH THE APPROPRIATE ADMINISTRATION REPRESENTATIVES TO COORDINATE WITH THE APPROPRIATE REGULATORY AGENCIES TO ENSURE THAT ALL PERMIT CONDITIONS ARE MET PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES WITHIN SPECIFIED SENSITIVE AREAS. SENSITIVE AREAS INCLUDE BUT ARE NOT LIMITED TO FLOODPLAINS, WETLANDS, WETLAND BUFFERS, CHESAPEAKE BAY CRITICAL AREA, FORESTS, ARCHEOLOGICAL SITES, HISTORIC SITES, PARKLAND, AND OPEN WATERS. DESIGNATE A RESPONSIBLE PARTY TO MONITOR ALL WORK IN THESE AREAS AND ENSURE THAT REASONABLE CARE IS TAKEN DURING WORK IN AND ADJACENT TO THESE AREAS.

8. INGRESS / EGRESS CONTROLS

PROTECT ALL POINTS OF CONSTRUCTION INGRESS AND EGRESS AND PREVENT THE DEPOSITION OF MATERIALS ON PUBLIC ROADS. IF DEPOSITION OCCURS, MECHANICALLY REMOVE ALL MATERIALS DEPOSITED ON PUBLIC ROADS IMMEDIATELY. FLUSHING OF ROAD SURFACES IS PROHIBITED. WHEN NO SCE IS PROVIDED, KEEP ALL CONSTRUCTION EQUIPMENT WITHIN THE LOD UNTIL THE WORK IS COMPLETE. CLEAN TREADS/TIRES PRIOR TO THE EQUIPMENT LEAVING THE LOD.

9. EROSION AND SEDIMENT CONTROL EXCAVATION

DISPOSE OF MATERIAL REMOVED FROM ESC DEVICES IN AN APPROVED WASTE SITE AS SPECIFIED IN SECTION 201. MATERIALS MAY BE STORED FOR RE-USE. MATERIALS STORED ON-SITE MAY BE REUSED ONCE IT IS DRIED AND IF IT MEETS THE REQUIREMENTS FOR EMBANKMENTS OR OTHER UNSPECIFIED NEEDS.

10. DEWATERING PRACTICES

OPERATE DEWATERING PRACTICES IN A MANNER THAT DOES NOT DISCHARGE SEDIMENT INTO WATERWAYS. NO VISIBLE CHANGES TO STREAM CLARITY ARE ACCEPTABLE.

11. STANDARD STABILIZATION NOTE

FOLLOWING INITIAL SOIL DISTURBANCE OR REDISTURBANCE, COMPLETE PERMANENT OR TEMPORARY STABILIZATION WITHIN THREE (3) CALENDAR DAYS FOR SURFACES OF ALL PERIMETER CONTROLS, DIKES, SWALES, DITCHES, PERIMETER SLOPES, AND ALL SLOPES GREATER THAN 3 HORIZONTAL TO 1 VERTICAL (3:1); AND SEVEN (7) DAYS AS TO ALL OTHER DISTURBED OR GRADED AREAS ON THE SITE. ENSURE CONTINUED STABILIZATION.

12. INCREMENTAL STABILIZATION

REFER TO THE MDE "2011 MARYLAND STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" FOR THE INCREMENTAL STABILIZATION OF CUT AND FILLS.

13. SEDIMENT TRAPS AND BASINS

PLAN DIMENSIONS ARE RELATIVE TO THE OUTLET ELEVATION. MAINTAIN INFLOW AND OUTFLOW LOCATIONS FOR TRAPS AND BASINS IN STABLE CONDITION.

14. OFF-SITE UTILITY WORK

FOLLOW ADDITIONAL BEST MANAGEMENT ESC PRACTICES FOR UTILITY CONSTRUCTION IN AREAS OUTSIDE OF DESIGNED CONTROLS:

- CALL "MISS UTILITY" AT 1-800-257-7777 AT LEAST 48 HOURS PRIOR TO THE START OF WORK.
- PLACE EXCAVATED MATERIAL ON THE HIGH SIDE OF TRENCHES.
- BACKFILL, COMPACT, AND STABILIZE AT THE END OF EACH WORKING DAY ALL TRENCHES FOR UTILITY INSTALLATIONS. WHEN THIS IS NOT POSSIBLE, CONFORM TO (d).
- PLACE TEMPORARY SILT FENCES IMMEDIATELY DOWNSTREAM OF ANY DISTURBED AREA THAT IS INTENDED TO REMAIN DISTURBED FOR MORE THAN ONE (1) DAY.

15. SITE INFORMATION*

- A. TOTAL AREA DISTURBED 0.54 ACRES
 B. TOTAL CUT 3 CU. YDS.
 C. TOTAL FILL 0 CU. YDS.
 D. OFFSITE WASTE/BORROW AREA LOCATION (IF KNOWN) N/A

* (NOT FOR BIDDING PURPOSES)

16. MODIFICATIONS

SUBMIT MODIFICATIONS OF THE ESC MEASURES OR PLAN TO THE ADMINISTRATION FOR APPROVAL. OBTAIN ALL APPROVALS PRIOR TO IMPLEMENTING ANY MODIFICATION.

STANDARD SYMBOLS			
100-YEAR FLOODPLAIN		MEDIAN INLET PROTECTION	
AT-GRADE INLET PROTECTION		MEDIAN SUMP INLET PROTECTION	
BAFFLE BOARDS		MOUNTABLE BERM	
BENCHING		PERIMETER DIKE/SWALE	
CATCH BASIN INSERT		PERMANENT SOIL STABILIZATION MATTING-TYPE B	
CHESAPEAKE BAY CRITICAL AREA		PERMANENT SOIL STABILIZATION MATTING-TYPE C	
CLEAR WATER DIVERSION PIPE		PIPE OUTLET SEDIMENT TRAP ST I	
CLEAR WATER PIPE		PIPE SLOPE DRAIN	
COMBINATION INLET PROTECTION		PLUNGE POOL	
CONCRETE WASHOUT STRUCTURE		PORTABLE SEDIMENT TANK	
CURB INLET PROTECTION		PROPOSED CONTOURS	
DIVERSION FENCE		REMOVABLE PUMPING STATION	
DRAINAGE BOUNDARY		RIPRAP INFLOW PROTECTION	
EARTH DIKE		RIPRAP OUTLET SEDIMENT TRAP ST III	
EMERGENCY SPILLWAY		ROCK OUTLET PROTECTION I	
EXISTING CONTOURS		ROCK OUTLET PROTECTION II	
FILTER BAG		ROCK OUTLET PROTECTION III	
FILTER BERM		SILT FENCE	
FILTER LOG		SILT FENCE ON PAVEMENT	
GABION INFLOW PROTECTION		SOD	
GABION INLET PROTECTION		STABILIZED CONSTRUCTION ENTRANCE (SCE)	
HORIZONTAL DRAW-DOWN DEVICE		STANDARD INLET PROTECTION	
LIMIT OF DISTURBANCE		STOCKPILE AREA	
		STONE CHECK DAM	
		STONE/RIPRAP OUTLET SEDIMENT TRAP ST II	
		SUBSURFACE DRAINS	
		SUMP PIT	
		SUPER SILT FENCE	
		TEMPORARY ACCESS BRIDGE	
		TEMPORARY ACCESS CULVERT	
		TEMPORARY ASPHALT BERM	
		TEMPORARY BARRIER DIVERSION	
		TEMPORARY GABION OUTLET STRUCTURE	
		TEMPORARY ORANGE CONSTRUCTION FENCE (TOCF)	
		TEMPORARY SOIL STABILIZATION MATTING-TYPE A	
		TEMPORARY SOIL STABILIZATION MATTING-TYPE E	
		TEMPORARY SOIL STABILIZATION MATTING-TYPE D	
		TEMPORARY STONE OUTLET STRUCTURE	
		TEMPORARY SWALE	
		TREE PROTECTION FENCE	
		VERTICAL DRAW-DOWN DEVICE	
		WASH RACK OPTION	
		WETLAND	
		WETLAND BUFFER	

MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
 MODIFICATION 1 APPROVAL

Walter Han 3/12/2022
 Chief, Plan Review Division
 PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

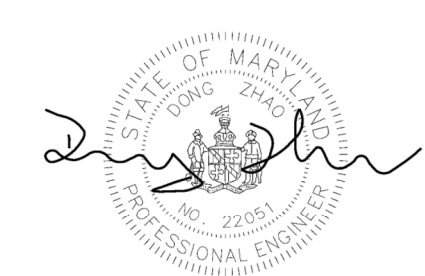
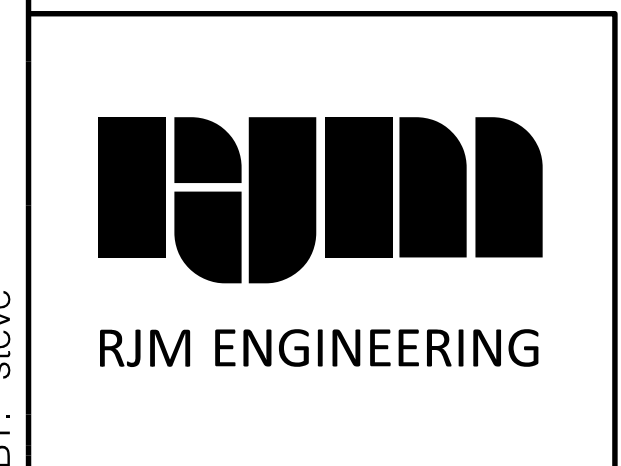
MDOT SHA STORMWATER MANAGEMENT & SEDIMENT CONTROL
 FINAL APPROVAL

Walter Han 3/12/2022
 Chief, Plan Review Division
 PRD No. 20-PR-0006-03 Expiration Date: 03/02/2025

INNOVATIVE CONTRACTING DIVISION
 ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM
 WEST OF SERVICE LANE TO EAST
 OF GOVERNOR ODEN BOWIE DRIVE

STATE HIGHWAY
 ADMINISTRATION



P.E. CERTIFICATION

I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND.

LICENSE NO. 22051

EXPIRATION DATE: 07/25/2022

DESIGN CERTIFICATION

I HEREBY CERTIFY THAT THIS PLAN HAS BEEN DESIGNED IN ACCORDANCE WITH THE MARYLAND STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, THE 2000 MARYLAND STORMWATER DESIGN MANUAL, VOLUMES I & II INCLUDING SUPPLEMENTS, THE ENVIRONMENT ARTICLE SECTIONS 4-101 THROUGH 116 AND SECTIONS 4-201 AND 215, AND THE CODE OF MARYLAND REGULATIONS (COMAR) 26.17.01 AND COMAR 26.17.02 FOR EROSION AND SEDIMENT CONTROL AND STORMWATER MANAGEMENT, RESPECTIVELY.

DATE 12/20/2021 DESIGNER'S SIGNATURE *Dong Zhao*

MD REGISTRATION NO. 22051 PRINTED NAME DONG ZHAO, P.E.

(P.E.) R.L.S., R.L.A., OR R.A. (CIRCLE ONE)

EROSION AND SEDIMENT CONTROL GENERAL NOTES	
SCALE <u>NTS</u> DATE <u>APRIL 12, 2022</u> CONTRACT NO. <u>PGAT15133</u>	
DESIGNED BY _____ COUNTY <u>PRINCE GEORGE'S</u>	
DRAWN BY _____ LOGMILE _____	
CHECKED BY _____ HORIZONTAL SCALE <u>N/A</u>	
MDE/PRD _____ VERTICAL SCALE <u>N/A</u>	
DRAWING NO. ES-01 OF 01 SHEET NO. <u>14</u> OF <u>29</u>	

BY: steve

CRITERIA

THE CONTRACTOR SHALL BE GOVERNED BY THE STANDARDS AND REQUIREMENTS OF THE FOLLOWING PUBLICATIONS, EXCEPT AS MODIFIED BY THE SPECIAL PROVISIONS OF THIS CONTRACT:

DESIGN

MDOT SHA - "MARYLAND MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", 2011 EDITION AND SUBSEQUENT REVISIONS. (MDMUTCD)

A A S H T O - "HIGHWAY SAFETY DESIGN AND OPERATIONS GUIDE" -1997

A A S H T O - "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS LUMINAIRES AND TRAFFIC SIGNALS", 2001 EDITION (CATEGORY II FOR ALL OVERHEAD AND CANTILEVER SIGN STRUCTURES).

MATERIALS AND CONSTRUCTION

MDOT SHA - "STANDARD SPECIFICATIONS FOR CONSTRUCTION & MATERIALS", MOST CURRENT EDITION AND SUBSEQUENT REVISIONS AND SUPPLEMENTS.

MDOT SHA - "BOOK OF STANDARDS FOR HIGHWAY AND INCIDENTAL STRUCTURES", MOST CURRENT EDITION AND SUBSEQUENT REVISIONS AND SUPPLEMENTS.

DESIGN WIND

100 MPH - WOOD SUPPORTS 10 YEAR RECURRENCE INTERVAL	} ALL DISTRICTS
100 MPH - GROUND MOUNT SIGN STEEL SUPPORTS 10 YEAR RECURRENCE INTERVAL	
100 MPH - OVERHEAD AND CANTILEVER STRUCTURES 50 YEAR RECURRENCE INTERVAL	

DESIGN STRESS

SOIL BEARING PRESSURE - S = 3,000 P.S.F. (ASSUMED)
SEE MATERIAL & CONSTRUCTION ABOVE AND SPECIAL PROVISIONS FOR DESIGN STRESSES FOR STRUCTURAL STEEL, ALUMINUM, REINFORCING STEEL AND CONCRETE.

CHAMFER

ALL EXPOSED EDGES OF CONCRETE SHALL HAVE A 3/4" X 3/4" CHAMFER.

CLASSIFICATION OF SIGNS

- SIGNS ARE DIVIDED INTO TWO (2) GENERAL CATEGORIES.
- I. GUIDE SIGNS
 - A) STRUCTURAL TYPES
 - OH - OVERHEAD
 - C - CANTILEVER
 - GM - GROUND MOUNT, BREAKAWAY OR NON-BREAKWAY
 - BM - BRIDGE MOUNTED
 - B) PANELS
 - MATERIAL - EXTRUDED ALUMINUM COPY - DIRECT APPLIED
 - I) HIGH INTENSITY (NEW SIGNS AND REVISIONS TO EXISTING SIGNS)
 - 2. STANDARD SIGNS (REGULATORY, WARNING, ETC.)
 - A) STRUCTURAL TYPES
 - WOOD SUPPORTS
 - SQUARE TUBE
 - B) PANELS
 - MATERIAL - SHEET ALUMINUM COPY - DIRECT APPLIED

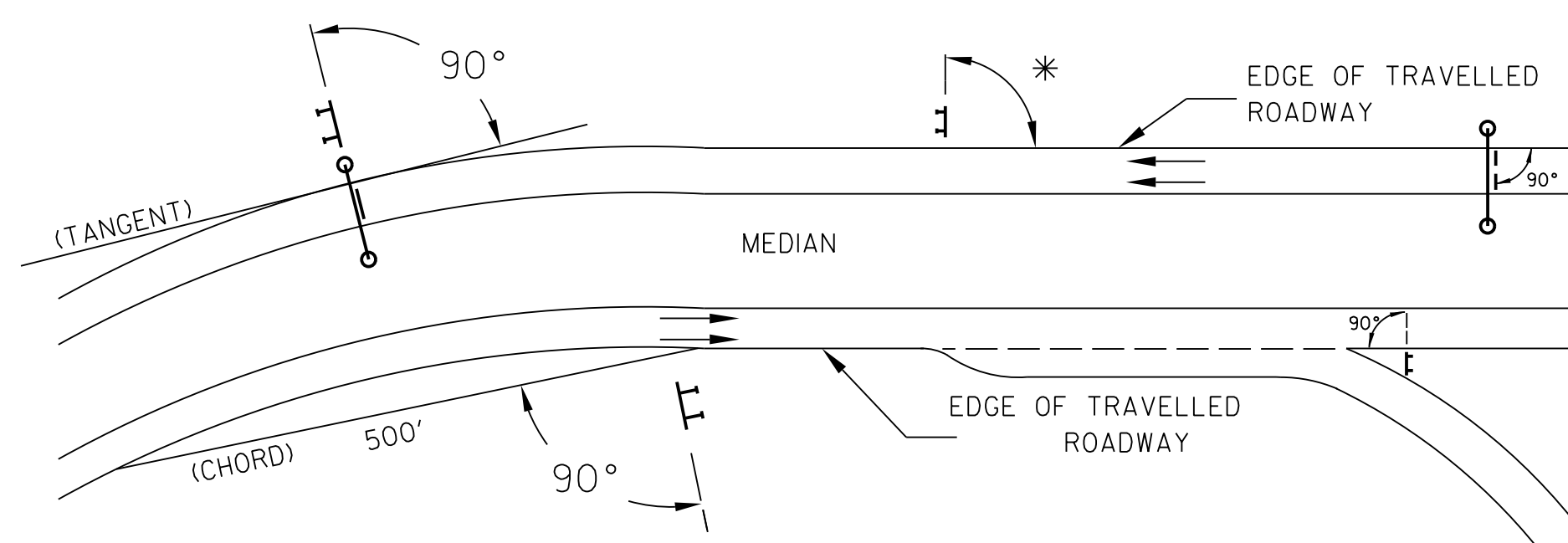
IDENTIFICATION OF SIGNS AND PANELS

- GUIDE SIGNS**
EACH GUIDE SIGN IS IDENTIFIED BY A SIGN NUMBER ON THE PLANS AND IN THE TABULATIONS. (GM-1, GM-2, GM-3, etc)
SIGNS ON STRUCTURES ARE IDENTIFIED WITH A NUMBER AND WHERE VARIATIONS OCCUR, A LOWER CASE LETTER. (OH-1a, OH-1b, OH-1c)
- STANDARD SIGNS**
STANDARD SIGNS ARE IDENTIFIED BY PANEL NUMBERS AND ARE CLASSIFIED AS FOLLOWS
R - REGULATORY
W - WARNING
M - ROUTE MARKERS AND ACCESSORIES
D - DESTINATION AND MILEAGE PANELS
S - SCHOOL
- PANELS SHALL BE DESIGNATED TO AGREE WITH MARYLAND STANDARD SIGN BOOK. EACH STANDARD SIGN IS IDENTIFIED FIRST BY THE SHEET NUMBER, THEN BY THE NUMERICAL ORDER OF THE SIGN AS IT APPEARS ON THE PLAN.
FOR EXAMPLE SHEET SN 2.1-101,102,103, ETC. SHEET SN 2.2-201,202,203,ETC.

PANEL LAYOUT AND ALPHABETS

- GUIDE SIGN PANEL LAYOUTS ARE BASED ON THE A.A.S.H.T.O. MANUALS NOTED ABOVE.
- STANDARD SIGN PANEL LAYOUTS ARE BASED ON THE MDMUTCD WITH SPECIFICATIONS DETAILED IN THE MARYLAND STATE HIGHWAY ADMINISTRATION PUBLICATION, "STANDARD SIGN BOOK", AVAILABLE ONLINE AT http://apps.roads.maryland.gov/businesswithsha/bizstdsspecs/desmanualstdpub/publicationsonline/oos/internet_signbook.asp

ORIENTATION OF SIGN FACES



* UNDER 30 FEET FROM TRAVELLED ROADWAY TO NEAR EDGE OF SIGN - 93° AWAY FROM THE ROAD TO AVOID SPECULAR REFLECTION AS INDICATED IN 813.03 OF THE MARYLAND STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS.
OVER 30 FEET FROM TRAVELLED ROADWAY TO NEAR EDGE OF SIGN - 90°

REFLECTORIZATION

BACKGROUNDS, BORDERS, TEXTS AND ALL OTHER ELEMENTS OF SIGN PANELS SHALL BE REFLECTORIZED EXCEPT WHERE NOTED. REFER TO PROJECT REQUIREMENTS FOR MORE DETAIL.

SIGN LOCATIONS

- GUIDE SIGNS ARE LOCATED ON THE PLANS BY DIMENSION TO SURVEY STATIONS, OR WHEN NECESSARY, TO IDENTIFIABLE PHYSICAL FEATURES.
- ALL CHANGES IN THE LOCATIONS OF SIGNS AS SHOWN ON THE PLAN SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

EXISTING UTILITIES

THE ENGINEER DOES NOT WARRANT OR GUARANTEE THE ACCURACY OR COMPLETENESS OF UTILITY INFORMATION SHOWN ON THE PLAN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ALL EXISTING FACILITIES WHICH MIGHT BE AFFECTED BY THIS WORK OR HIS OPERATION.

ROADSIDE SIGNS

- VERTICAL ALIGNMENT
POSITION PANEL SO FACE IS PLUMB.
- HORIZONTAL ALIGNMENT (SEE DIAGRAM ABOVE)
 - A) ON STRAIGHT ROADWAY SECTIONS, ANGLE OF SIGN FACE TO ROADWAY VARIES WITH DISTANCE FROM TRAVELLED ROADWAY TO NEAR EDGE OF SIGN - SEE DIAGRAM.
 - B) ON THE INSIDE OF HORIZONTAL CURVES, POSITION SIGN SO FACE OF PANEL MAKES AN ANGLE OF 90° WITH A CHORD BETWEEN A POINT ON NEAR EDGE OF PAVEMENT AT SIGN LOCATION AND A POINT ON EDGE OF PAVEMENT 500' IN ADVANCE OF SIGN.
 - C) ON THE OUTSIDE OF HORIZONTAL CURVES, POSITION SIGN SO FACE OF PANEL IS AT RIGHT ANGLES TO THE TANGENT OF THE CURVE AT THE SIGN LOCATION.
 - D) POSITIONING OF SIGNS AT GORES AND RAMP SEPARATIONS IS REFERRED TO THE NORMAL EDGE OF THE MAINLINE ROADWAY.

OVERHEAD SIGNS

- VERTICAL ALIGNMENT
POSITION PANELS FOR ALL OVERHEAD STRUCTURES SO THAT PANEL FACE IS PLUMB.
- OVERHEAD SIGN STRUCTURES SHALL NOT BE ERECTED WITHOUT ATTACHING LUMINAIRES, SUPPORTS, AND/OR SIGNS.
- HORIZONTAL ALIGNMENT
 - A) POSITION ALL OVERHEAD SIGNS SO THAT THE FACE OF THE PANEL IS AT RIGHT ANGLES TO THE NORMAL EDGE OF ROADWAY, IF ON A STRAIGHT ROADWAY SECTION.
 - B) POSITION ALL OVERHEAD SIGNS SO THAT THE FACE OF THE PANEL IS AT RIGHT ANGLES TO THE TANGENT OF THE CURVE AT SIGN LOCATION, IF ON A HORIZONTAL CURVE.
 - C) POSITIONING OF SIGNS AT GORES AND RAMP SEPARATIONS IS REFERRED TO THE NORMAL EDGE OF THE MAINLINE ROADWAY.
- VERTICAL CLEARANCE
 - A) OVERHEAD SIGNS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 17'-9" FROM ROADWAY TO THE BOTTOM OF LIGHT FIXTURES. ALL LIGHT FIXTURES ARE TO BE AT THE SAME ELEVATION.
 - B) IF THE CONTRACTOR CANNOT OBTAIN 17'-9" (SEE 3A) CLEARANCE, HE IS TO CEASE WORK AND CONTACT THE PROJECT ENGINEER FOR FURTHER INSTRUCTIONS. THE PROJECT ENGINEER MAY CONTACT THE TRAFFIC ENGINEERING DESIGN DIVISION FOR ASSISTANCE.
 - C) ON ALL OVERHEAD SIGNS, THE MINIMUM CLEARANCE TO BOTTOM OF DESIGN SIGN: 20'-9".

PROJECT REQUIREMENTS

ALL NEW SIGNS ON THIS PROJECT SHALL BE FABRICATED FROM SHEETING WHICH MEETS ALL OF THE FOLLOWING REQUIREMENTS, UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, OR AS DIRECTED BY THE ENGINEER:

- SHEETING SHALL MEET THE REQUIREMENTS OF SECTIONS 813 AND 950.03 OF MDOT SHA'S STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS MOST CURRENT EDITION AND SUBSEQUENT REVISIONS AND SUPPLEMENTS.
- LISTED ON MDOT SHA OFFICE OF TRAFFIC AND SAFETY'S QUALIFIED PRODUCTS LIST (QPL).

PROJECT REQUIREMENTS CONT'D

3. THE FOLLOWING TYPES OF SHEETING SHALL BE USED FOR THE SPECIFIED SIGN CLASSIFICATIONS:

GENERAL NOTE: ALL COLORS SHALL BE RETROREFLECTIVE EXCEPT BLACK. BLACK TEXT, BORDERS, SYMBOLS OR ANY BLACK ELEMENTS OF ANY SIGN SHALL BE NON-REFLECTIVE. THIS APPLIES TO ALL MDOT SHA SIGNS AS SHOWN BELOW.

A) GUIDE, EXIT GORE, GENERAL INFORMATION, AND SERVICE SIGNS - FALL INTO TWO SUB CATEGORIES:

- (I). GROUND MOUNTED:
ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9).
- (II). OVERHEAD STRUCTURE SIGNS AND OVERHEAD CANTILEVER SIGNS:
ALL RETROREFLECTIVE SHEETING ELEMENTS OF ALL OVERHEAD SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE XI(II). (THIS SECTION DOES NOT APPLY TO OVERHEAD SIGNALIZED INTERSECTION SIGNING; MAST ARM OR SPAN WIRE. FOLLOW THE REQUIREMENTS FOR THE RESPECTIVE SIGN CLASSIFICATION FOR SIGNAL SIGNING.)

B) WARNING SIGNS - RETROREFLECTIVE SHEETING FOR WARNING SIGNS (FLUORESCENT YELLOW AND FLUORESCENT ORANGE) SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9). REGULATORY MESSAGES WITHIN WARNING SIGNS SHALL FOLLOW THE REQUIREMENTS FOR REGULATORY SIGNS.

C) SCHOOL SIGNS - RETROREFLECTIVE SHEETING FOR SCHOOL SIGNS (FLUORESCENT YELLOW AND FLUORESCENT YELLOW-GREEN) SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9). REGULATORY MESSAGES WITHIN SCHOOL SIGNS SHALL FOLLOW THE REQUIREMENTS FOR REGULATORY SIGNS.

D) REGULATORY SIGNS - FALL INTO THREE SUBCATEGORIES:

- (I). "RED" REGULATORY SIGNS; (SPECIFICALLY - STOP, YIELD, DO NOT ENTER AND WRONG WAY). ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9).
- (II). ALL R7 AND R8 SERIES PARKING RELATED SIGNS AND THEIR SUPPLEMENTAL PANELS, NO TRESPASSING SIGNS, AND SIGNS DIRECTED AT PEDESTRIANS AND BICYCLISTS ONLY. ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET THE REQUIREMENTS FOR ASTM TYPE IV (4).
- (III). ALL OTHER REGULATORY SIGNS - ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET ASTM TYPE IV (4) INCLUDING RED ELEMENTS. WARNING MESSAGES WITHIN REGULATORY SIGNS SHALL FOLLOW THE REQUIREMENTS FOR WARNING SIGNS.

E) ROUTE MARKERS (INDEPENDENT USE AND GUIDE SIGN USE)

INDEPENDENT USE: ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET BUT NOT TO EXCEED THE REQUIREMENTS FOR ASTM TYPE IV (4).

GUIDE SIGN USE: WHEN INCORPORATED IN THE BODY OF A GUIDE SIGN, ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET THE SHEETING REQUIREMENTS OF THE GUIDE SIGNS FOR WHICH THEY ARE TO BE APPLIED; GROUND MOUNT ASTM TYPE IX (9) OR OVERHEAD ASTM TYPE XI(II).

F) LOGOS AND / OR GRAPHICS - WITHIN SIGNS SHALL FOLLOW THE REQUIREMENTS FOR THE RESPECTIVE SIGN CLASSIFICATION UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, OR AS DIRECTED BY THE ENGINEER.

G) SPECIFIC SERVICE (LOGO) SIGNING - ALL COPY, DIVIDER BORDERS, LOGOS AND ARROWS SHALL BE DEMOUNTABLE ALUMINUM OVERLAYS. .032 MINIMUM TO .063 MAXIMUM. ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9). DISTANCES ON DIRECTIONAL ARROWS WHEN SPECIFIED SHALL BE BLACK. THE OVERLAYS ARE TO BE APPLIED WITH .125 ALUMINUM POP RIVETS TO THE BODY OF THE MAIN SIGN.

H) CIVIL DEFENSE SIGNS AND OTHER SIGNS - NOT SPECIFICALLY FALLING INTO ONE OF THE CATEGORIES ABOVE, SHALL FOLLOW THE GUIDELINES FOR THE SIGN CLASSIFICATION THAT MOST CLOSELY MATCHES THE COLOR(S) OF THE PROPOSED SIGN.

4. THE FOLLOWING MINIMUM THICKNESS SHALL BE USED FOR THE APPROPRIATE WIDTH OF SHEET ALUMINUM BLANKS:

LONGEST DIMENSION	MINIMUM THICKNESS
UP TO 12".....	0.040"
GREATER THAN 12" TO 24".....	0.063"
GREATER THAN 24" TO 36".....	0.080"
GREATER THAN 36" TO 48".....	0.100"
OVER 48".....	0.125"

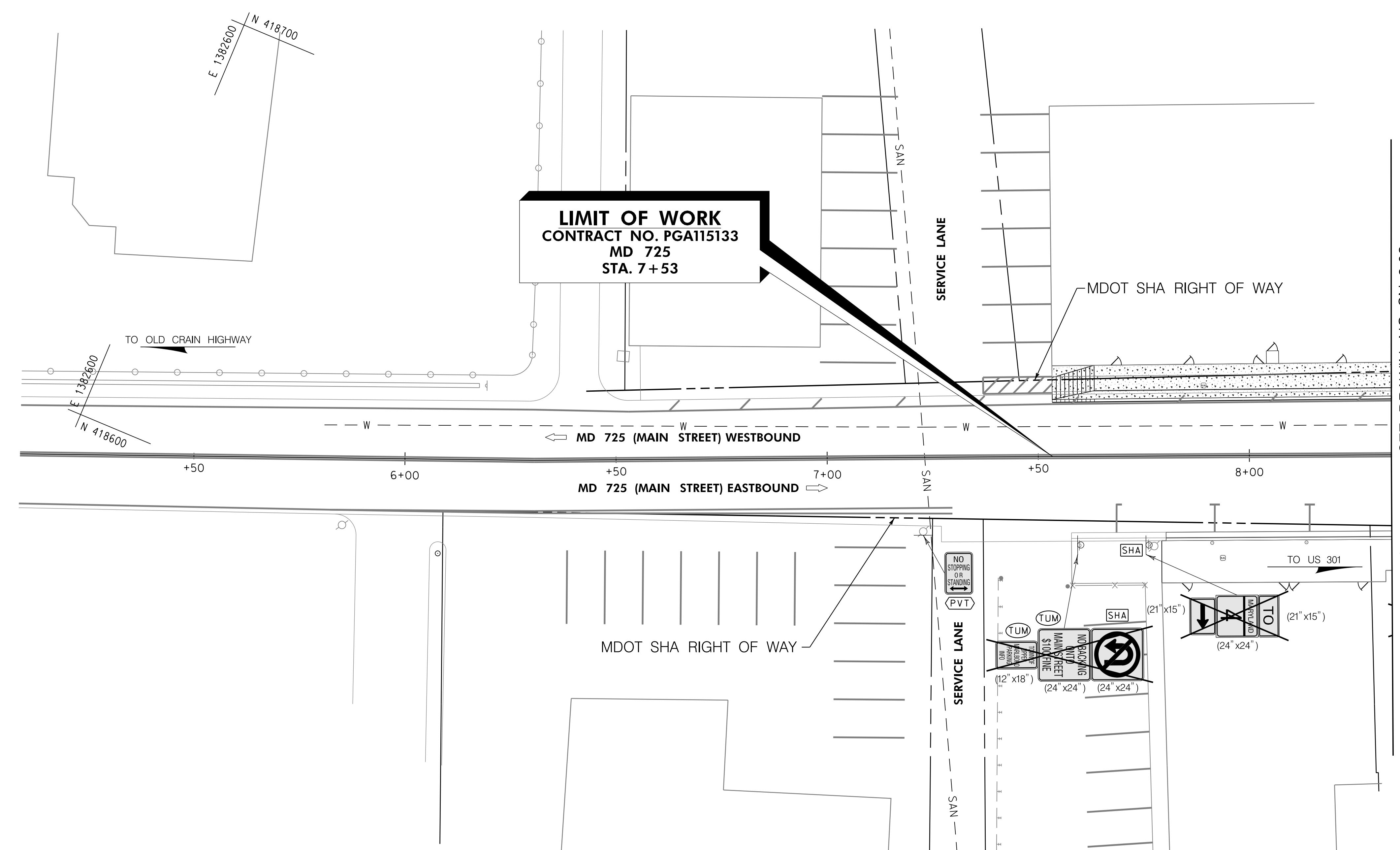
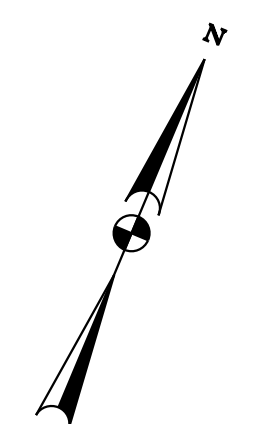
OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

APPROVALS	REVISIONS	GENERAL NOTES AND PROPOSALS	
Robert Stewart 01/11/22 <small>TEAM LEADER</small> Adiaon Joretic 1/12/2022 <small>ASST. DIV. CHIEF</small> Jialin Tian 1/31/2022 <small>DESIGN CHIEF</small> Cedric Ward 2/2/22 <small>OFFICE DIRECTOR</small>		SCALE _____ N.T.S. _____ DATE _____ APRIL 12, 2022 _____ CONTRACT NO. _____ PGAT15133 _____	
		DESIGNED BY _____ JHR _____ COUNTY _____ PRINCE GEORGE'S _____	
		DRAWN BY _____ JHR _____ LOGMILE _____ 16072500.59 - 16072501.13 _____	
		CHECKED BY _____ JMM _____ TMS NO. _____ PS31 _____	
		MDE/PRD _____ 20-PR-0006-03 _____ TOD NO. _____	
		DRAWING NO. SN-1 OF 1 SHEET NO. 15 OF 29	

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231



LIMIT OF WORK
CONTRACT NO. PG115133
MD 725
STA. 7+53

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

- GENERAL NOTES:**
- THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
 - CONTRACTOR SHALL CONTACT MR. DARNELL BOND, DIRECTOR OF PUBLIC WORKS FOR THE TOWN OF UPPER MARLBORO AT dbond@uppermarlboromd.gov OR 301-509-4756 A MINIMUM OF 7 DAYS PRIOR TO REMOVAL OF SIGNS NOTED AS OWNED BY THE TOWN TO COORDINATE RETURN OF THE SIGNS TO THE TOWN.
 - EXISTING SIDEWALK INCLUDING AREAS OF BRICK, DISTURBED BY SIGN SUPPORT REMOVAL SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER.

- PAVEMENT MARKING LEGEND**
- (A) 5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
 - (B) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
 - (C) REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

OFFICE OF TRAFFIC & SAFETY
 TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
 MARYLAND DEPARTMENT OF TRANSPORTATION
 STATE HIGHWAY ADMINISTRATION

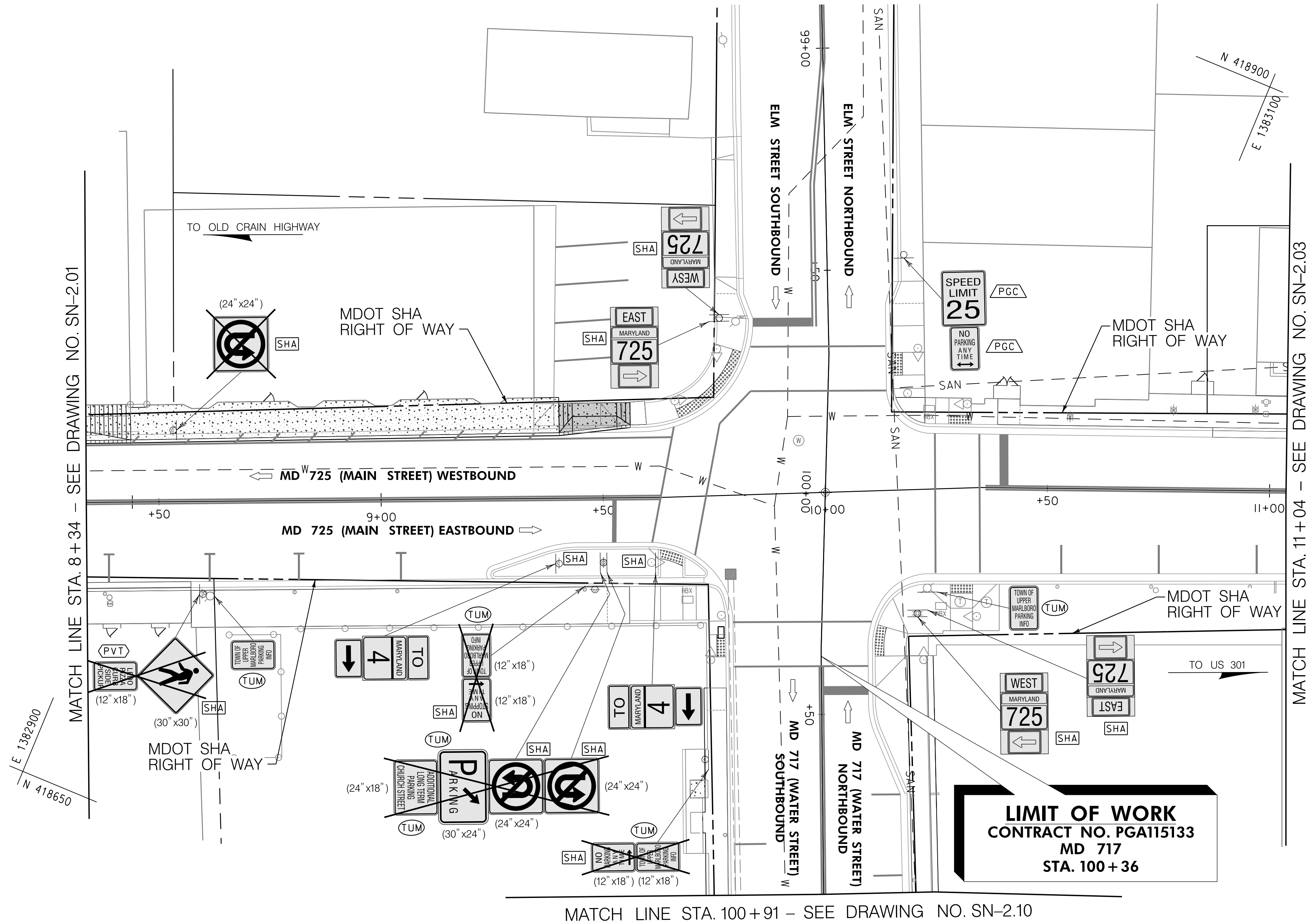
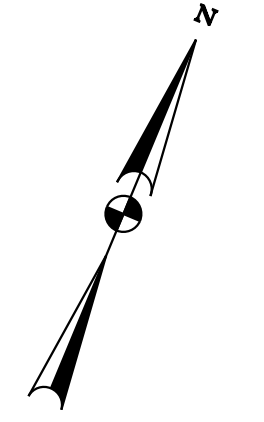
ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE

SIGNING AND MARKING PLAN	
SCALE	1" = 15'
DATE	APRIL 12, 2022
CONTRACT NO.	PG115133
DESIGNED BY	JHR
COUNTY	PRINCE GEORGE'S
DRAWN BY	JHR
LOGMILE	16072500.59 - 16072501.13
CHECKED BY	JMM
TMS NO.	PS31
MDE/PRD	20-PR-0006-03
TOD NO.	
DRAWING NO.	SN - 2.01 OF 11
SHEET NO.	16 OF 29

WRA
 Whitman, Requardt & Associates, LLP
 801 South Caroline Street, Baltimore, Maryland 21231

BY: bdonoway



GENERAL NOTES:

1. THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
2. CONTRACTOR SHALL CONTACT MR. DARNELL BOND, DIRECTOR OF PUBLIC WORKS FOR THE TOWN OF UPPER MARLBORO AT dbond@uppermarlboromd.gov OR 301-509-4756 A MINIMUM OF 7 DAYS PRIOR TO REMOVAL OF SIGNS NOTED AS OWNED BY THE TOWN TO COORDINATE RETURN OF THE SIGNS TO THE TOWN.
3. EXISTING SIDEWALK, INCLUDING AREAS OF BRICK, DISTURBED BY SIGN SUPPORT REMOVAL SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER.

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

PAVEMENT MARKING LEGEND	
(A)	5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
(B)	24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
(C)	REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

STATE HIGHWAY ADMINISTRATION

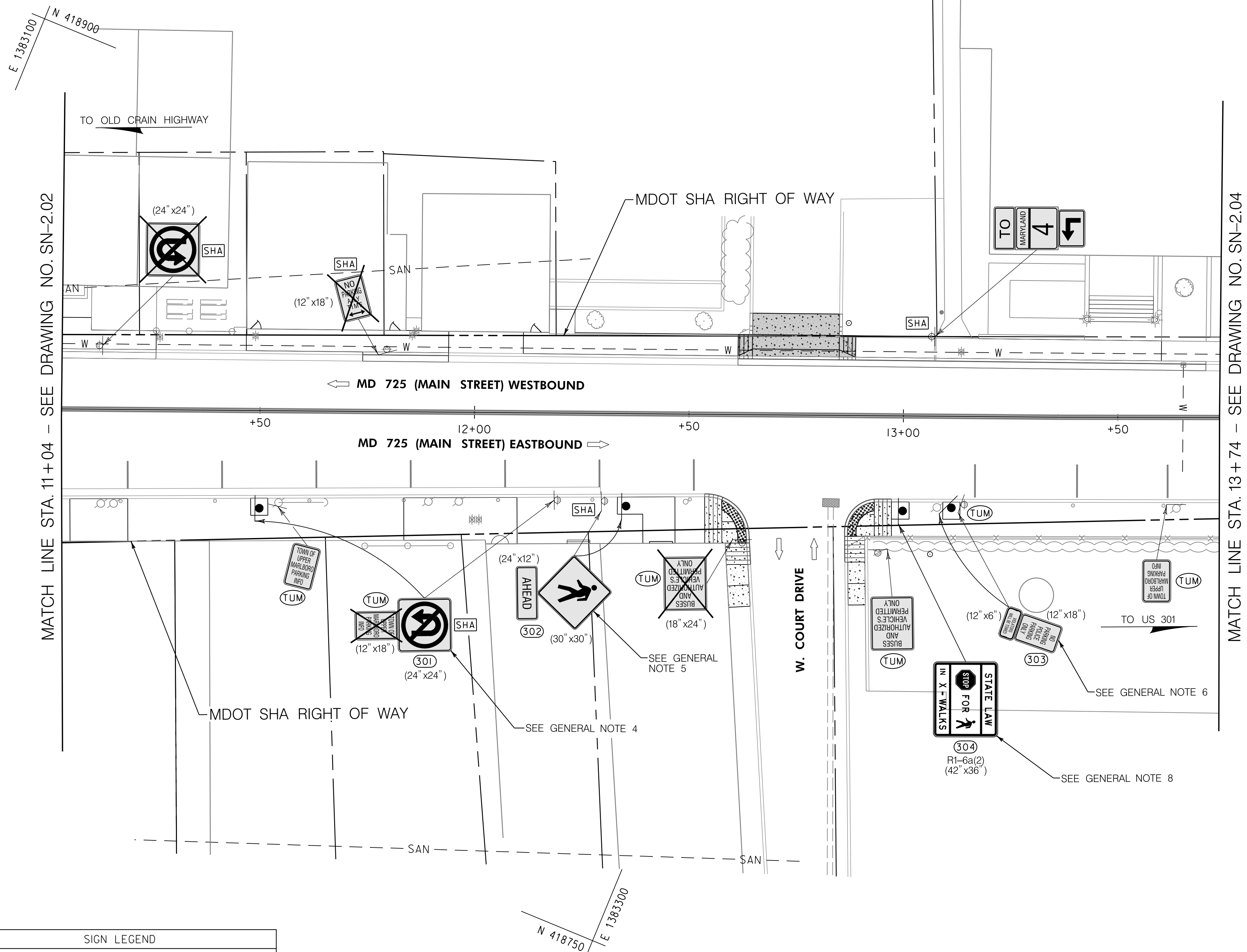
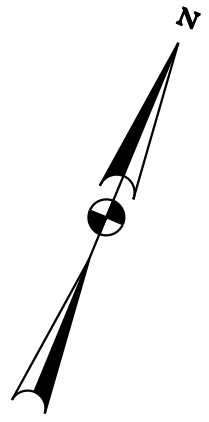
ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM
WEST OF SERVICE LANE TO EAST
OF GOVERNOR ODEN BOWIE DRIVE

SIGNING AND MARKING PLAN	
REVISIONS	SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TOD NO. PS31
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN - 2.02 OF 11	SHEET NO. 17 OF 29

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: bdonoway



MATCH LINE STA. 11+04 - SEE DRAWING NO. SN-2.02

MATCH LINE STA. 13+74 - SEE DRAWING NO. SN-2.04

GENERAL NOTES:

1. THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
2. CONTRACTOR SHALL CONTACT MR. DARNELL BOND, DIRECTOR OF PUBLIC WORKS FOR THE TOWN OF UPPER MARLBORO AT dbond@uppermarboromd.gov OR 301-509-4756 A MINIMUM OF 7 DAYS PRIOR TO REMOVAL OF SIGNS NOTED AS OWNED BY THE TOWN TO COORDINATE RETURN OF THE SIGNS TO THE TOWN.
3. FOR PROPOSED SIGN SUPPORTS SHOWN IN SIDEWALK, CONTRACTOR SHALL PROVIDE PVC INSERT PRIOR TO PLACING SIDEWALK. COST SHALL BE INCIDENTAL TO SIDEWALK.
4. REMOVE EXISTING "TOWN OF UPPER MARLBORO PARKING" SIGN AND SIGN SUPPORT ONLY. RELOCATE EXISTING "NO U-TURN" SIGN TO STATION 11+50 RIGHT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 1' OFFSET FROM FACE OF CURB TO LEFT EDGE OF SIGN, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.
5. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "PEDESTRIAN AHEAD" SIGNS TO STATION 12+35.5 RIGHT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 1' OFFSET FROM FACE OF CURB TO LEFT EDGE OF SIGN, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.
6. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "NO PARKING POLICE PARKING ONLY" AND "VIOLATORS WILL BE TOWED" SIGNS TO STATION 13+11.5 RIGHT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 1' OFFSET FROM FACE OF CURB TO LEFT EDGE OF SIGN, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.
7. EXISTING SIDEWALK, INCLUDING AREAS OF BRICK, DISTURBED BY SIGN SUPPORT REMOVAL SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER.
8. INSTALL PROPOSED SIGN ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 1' OFFSET FROM FACE OF CURB TO LEFT EDGE OF SIGN, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

PAVEMENT MARKING LEGEND	
(A)	5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
(B)	24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
(C)	REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

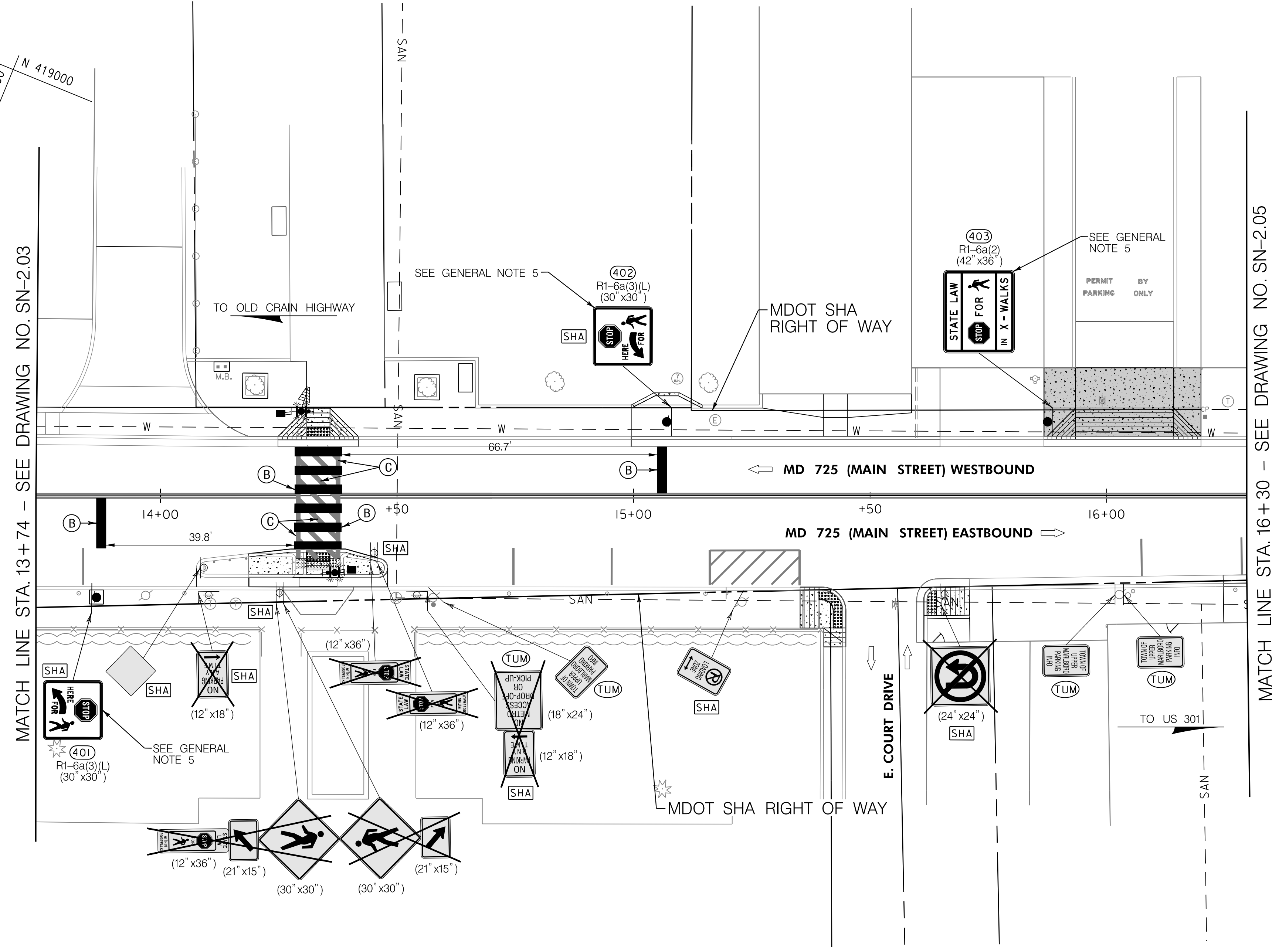
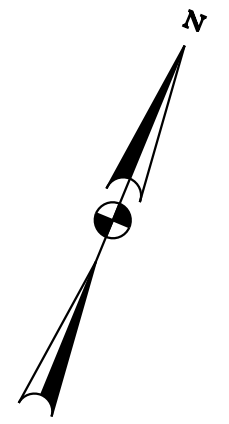
STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

SIGNING AND MARKING PLAN	
REVISIONS	SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. P531
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN - 2.03 OF 11	SHEET NO. 18 OF 29

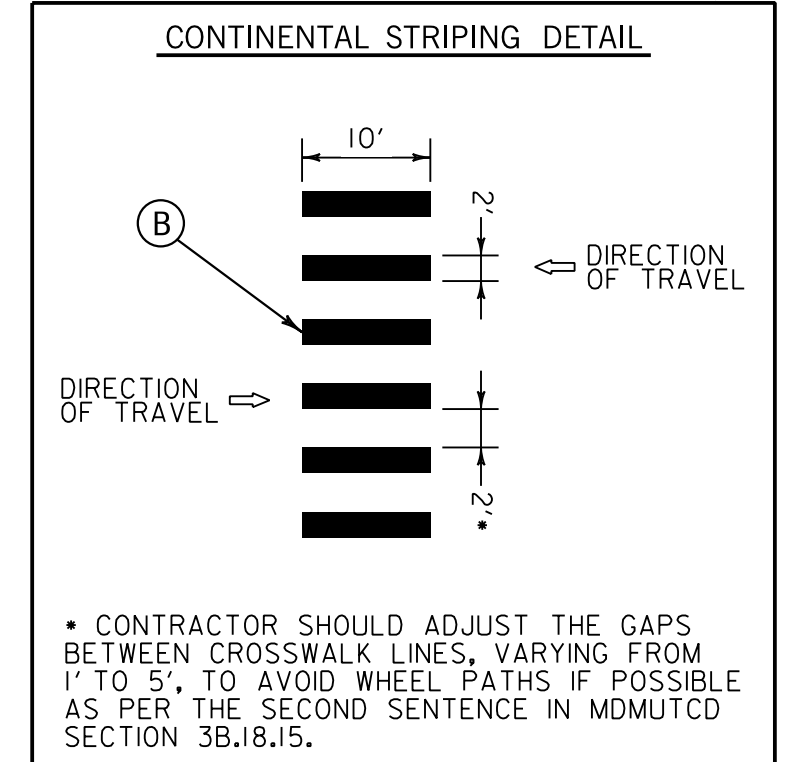
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: mknowles



MATCH LINE STA. 13+74 - SEE DRAWING NO. SN-2.03

MATCH LINE STA. 16+30 - SEE DRAWING NO. SN-2.05



E 1383350
N 418850

E 1383600
N 418850

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

GENERAL NOTES:

- THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
- CONTRACTOR SHALL CONTACT MR. DARNELL BOND, DIRECTOR OF PUBLIC WORKS FOR THE TOWN OF UPPER MARLBORO AT dbond@uppermarboromd.gov OR 301-509-4756 A MINIMUM OF 7 DAYS PRIOR TO REMOVAL OF SIGNS NOTED AS OWNED BY THE TOWN TO COORDINATE RETURN OF THE SIGNS TO THE TOWN.
- SEE SIGNAL PLANS FOR RECTANGULAR RAPID-FLASH BEACON AND PROPOSED PAVEMENT MARKING DETAILS.
- EXISTING SIDEWALK, INCLUDING AREAS OF BRICK, DISTURBED BY SIGN SUPPORT REMOVAL SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER.
- INSTALL PROPOSED SIGN ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 1' OFFSET FROM FACE OF CURB TO LEFT EDGE OF SIGN, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.

PAVEMENT MARKING LEGEND

- (A) 5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
- (B) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
- (C) REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

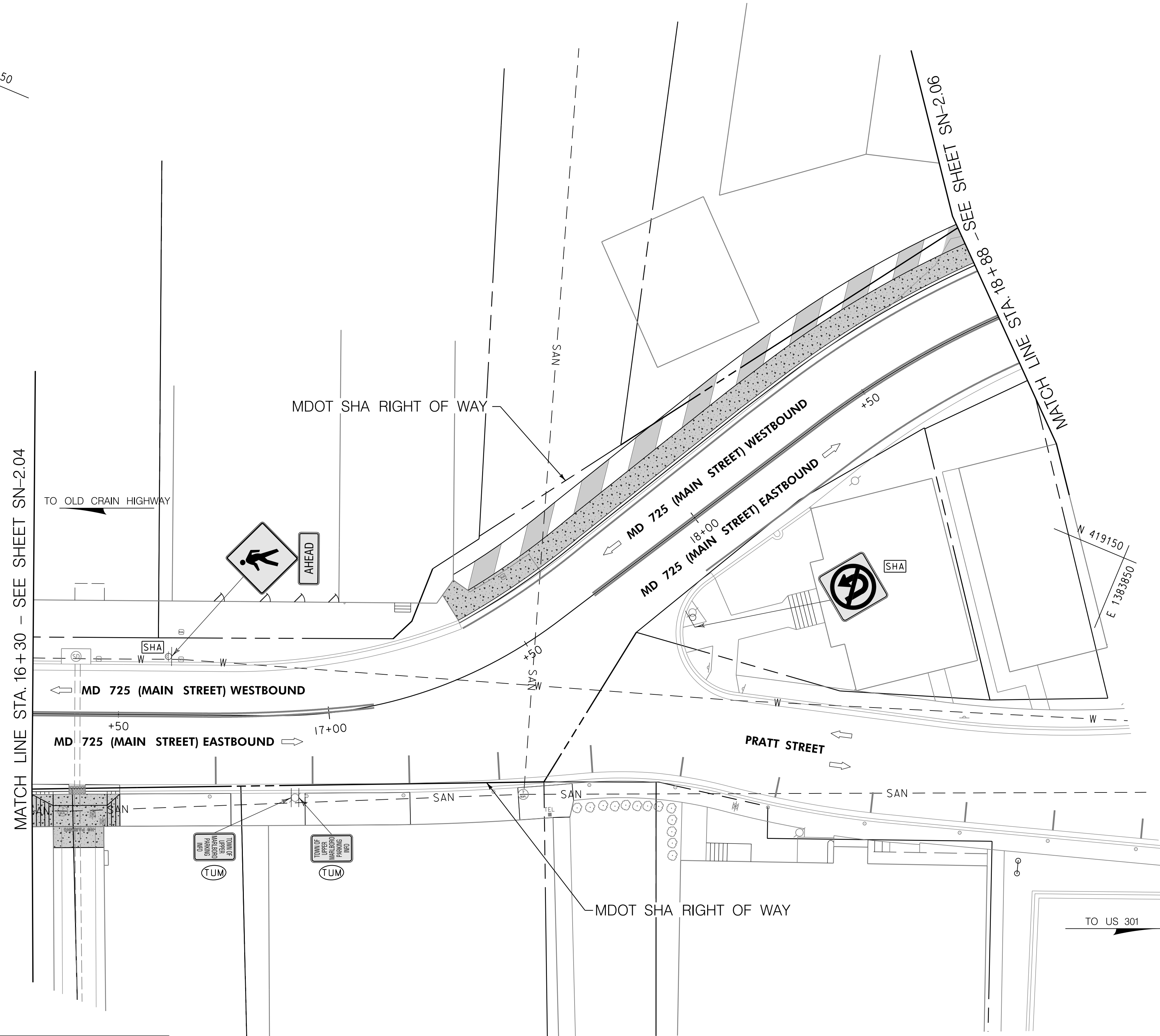
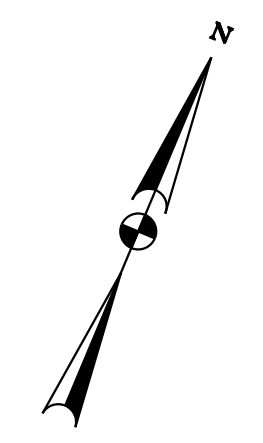
STATE HIGHWAY ADMINISTRATION

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE SIDEWALKS

SIGNING AND MARKING PLAN	
SCALE 1" = 15'	DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. PS31
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN - 2.04 OF 11	SHEET NO. 19 OF 29

BY: mknowles

E 1383550
N 419150



- PAVEMENT MARKING LEGEND**
- (A) 5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
 - (B) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
 - (C) REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

GENERAL NOTES:
1. THERE IS NO PROPOSED SIGNING AND MARKING WORK ON THIS PLAN SHEET.

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

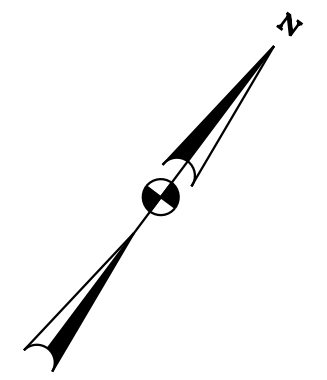
ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE

SIGNING AND MARKING PLAN	
SCALE 1" = 15'	DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. PS31
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN - 2.05 OF 11	SHEET NO. 20 OF 29

WRA
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

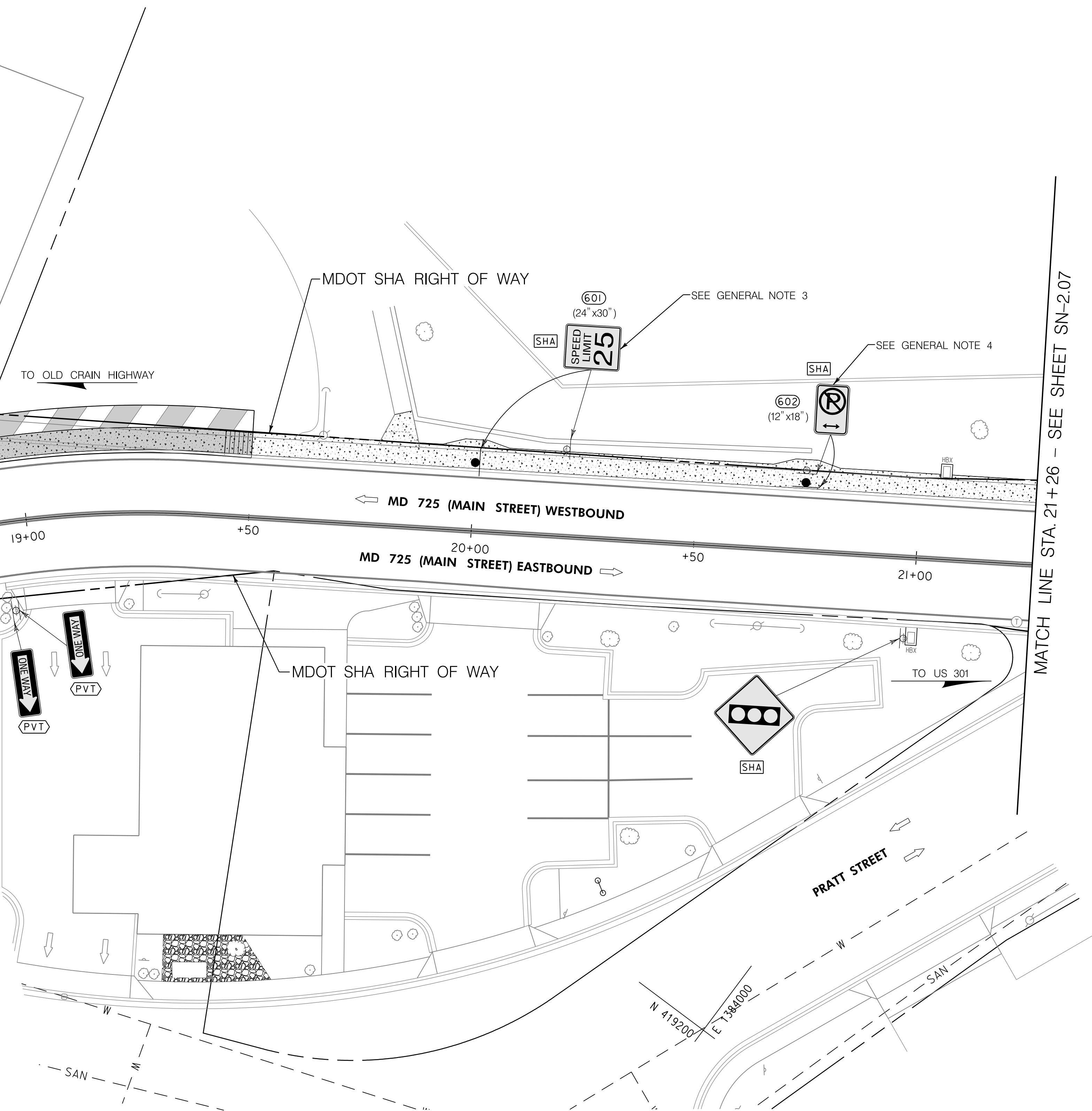
BY: bdonoway



E 1383700
N 419200

MATCH LINE STA. 18+88 - SEE SHEET SN-2.05

MATCH LINE STA. 21+26 - SEE SHEET SN-2.07



- GENERAL NOTES:**
1. THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
 2. CONTRACTOR SHALL CONTACT MR. DARNELL BOND, DIRECTOR OF PUBLIC WORKS FOR THE TOWN OF UPPER MARLBORO AT dbond@uppermarboromd.gov OR 301-509-4756 A MINIMUM OF 7 DAYS PRIOR TO REMOVAL OF SIGNS NOTED AS OWNED BY THE TOWN TO COORDINATE RETURN OF THE SIGNS TO THE TOWN.
 3. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "SPEED LIMIT 25 MPH" SIGN TO STATION 20+00 LEFT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 1' OFFSET FROM FACE OF CURB TO LEFT EDGE OF SIGN AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.
 4. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "NO PARKING" SIGN TO STATION 20+74.3 LEFT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 2' OFFSET FROM FACE OF CURB TO FACE OF SIGN ORIENTED PARALLEL TO FACE OF CURB, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.

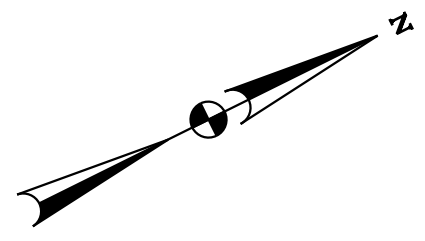
SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

- PAVEMENT MARKING LEGEND**
- (A) 5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
 - (B) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
 - (C) REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

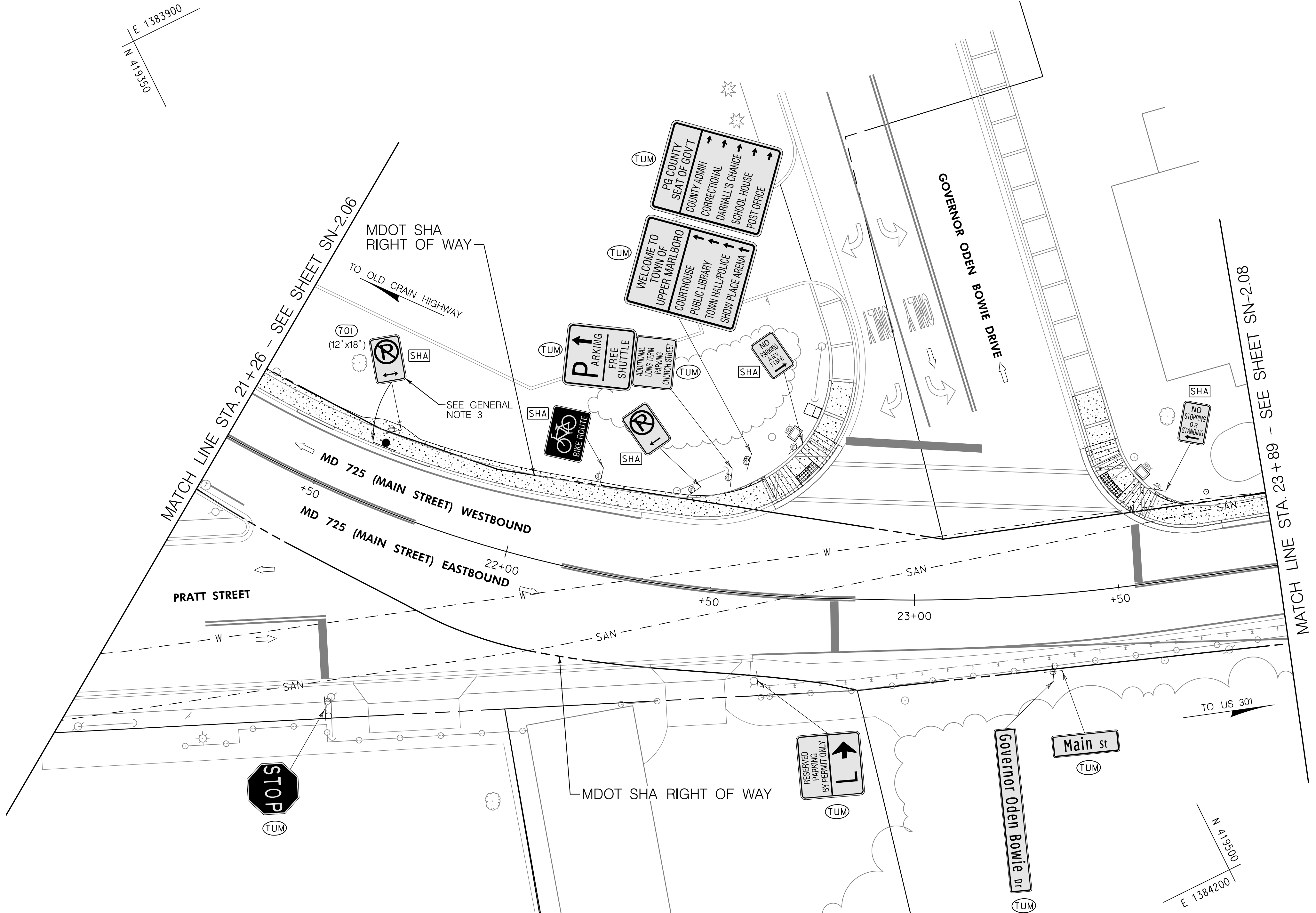
SIGNING AND MARKING PLAN	
SCALE 1" = 15'	DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. PS31
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN - 2.06 OF 11	SHEET NO. 21 OF 29

BY: bdonoway

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231



E 1383900
N 413350



MATCH LINE STA. 21+26 - SEE SHEET SN-2.06
MD 725 (MAIN STREET) WESTBOUND
MD 725 (MAIN STREET) EASTBOUND
PRATT STREET
MDOT SHA RIGHT OF WAY
TO OLD CRAIN HIGHWAY

MATCH LINE STA. 23+89 - SEE SHEET SN-2.08
GOVERNOR ODEN BOWIE DRIVE
TO US 301

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

- GENERAL NOTES:
1. THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
 2. CONTRACTOR SHALL CONTACT MR. DARNELL BOND, DIRECTOR OF PUBLIC WORKS FOR THE TOWN OF UPPER MARLBORO AT dbond@uppermarlboromd.gov OR 301-509-4756 A MINIMUM OF 7 DAYS PRIOR TO REMOVAL OF SIGNS NOTED AS OWNED BY THE TOWN TO COORDINATE RETURN OF THE SIGNS TO THE TOWN.
 3. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "NO PARKING" SIGN TO STATION 21+62.5 LEFT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 2' OFFSET FROM FACE OF CURB TO FACE OF SIGN ORIENTED PARALLEL TO FACE OF CURB AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.

- PAVEMENT MARKING LEGEND
- (A) 5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
 - (B) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
 - (C) REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

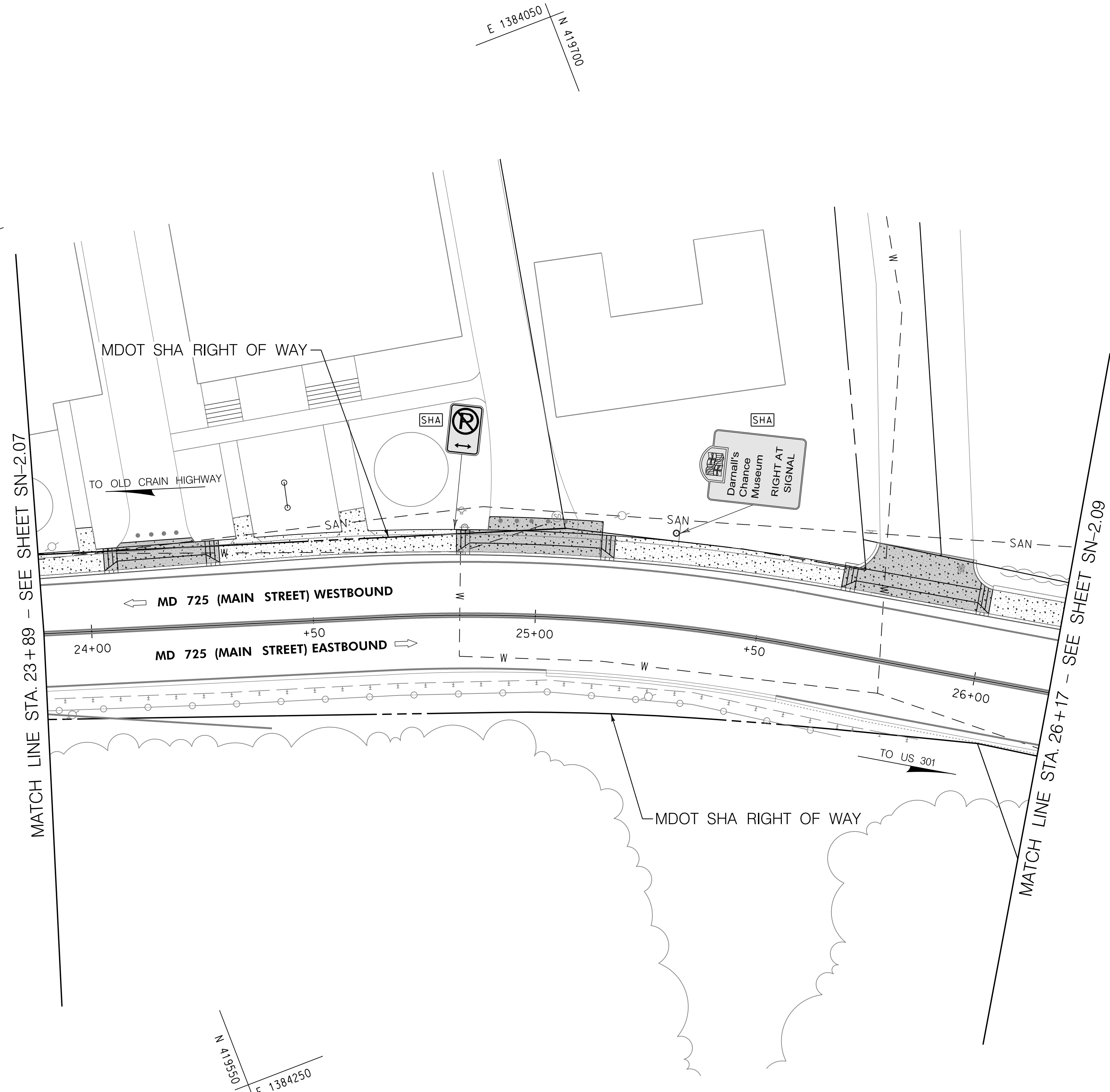
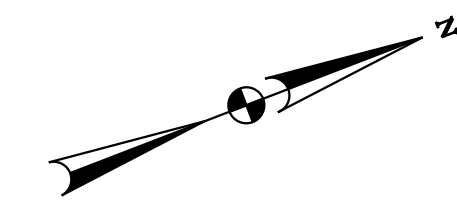
ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE

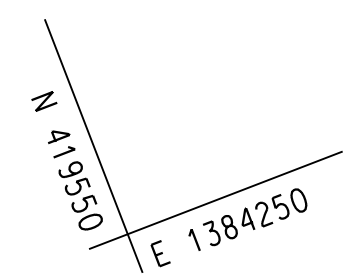
SIGNING AND MARKING PLAN	
SCALE 1" = 15'	DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGES
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. P531
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN - 2.07 OF 11	SHEET NO. 22 OF 29

WRA
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: bdonoway



SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY



- PAVEMENT MARKING LEGEND**
- (A) 5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
 - (B) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
 - (C) REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

GENERAL NOTES:

1. THERE IS NO PROPOSED SIGNING AND MARKING WORK ON THIS PLAN SHEET.

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

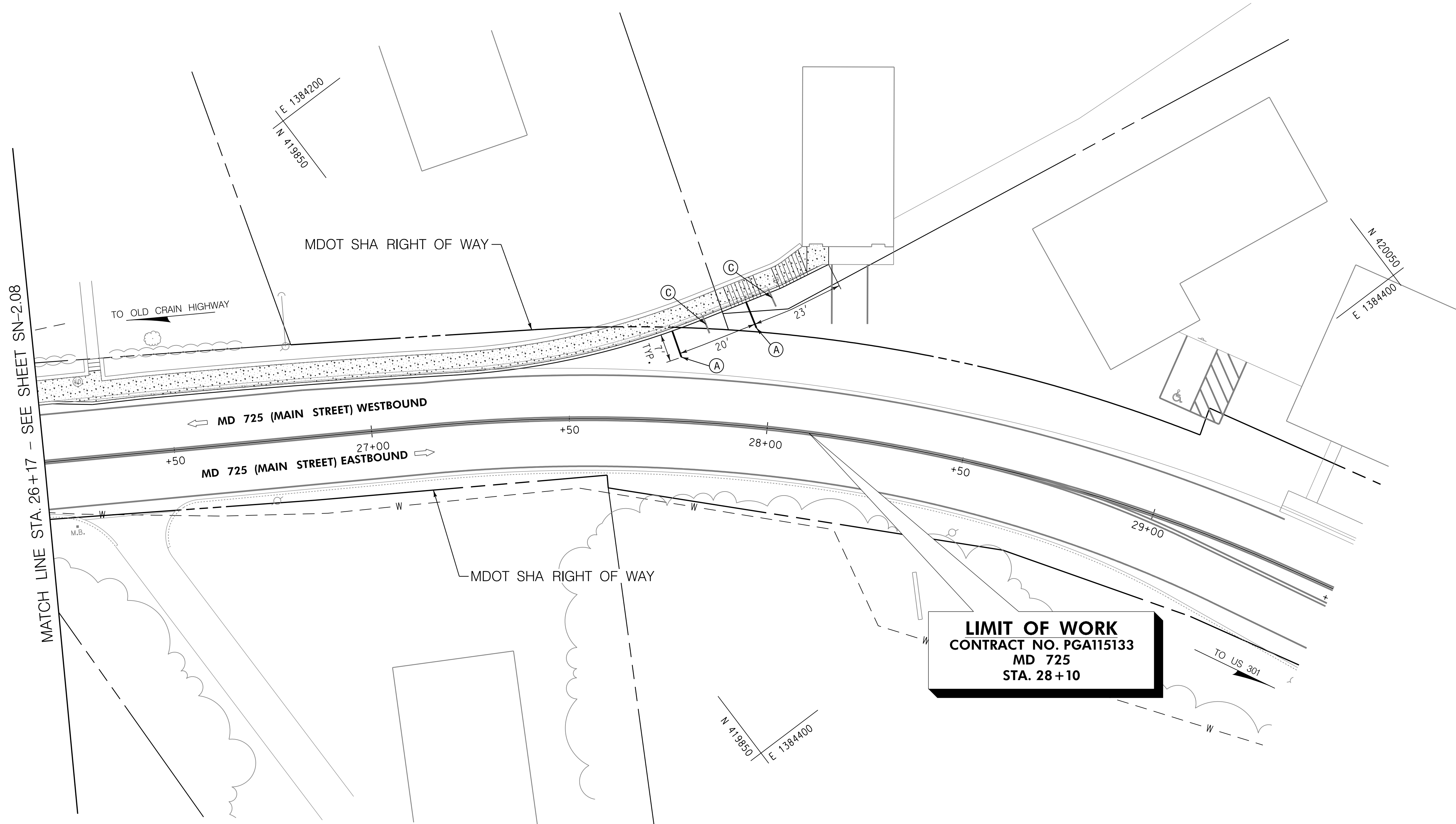
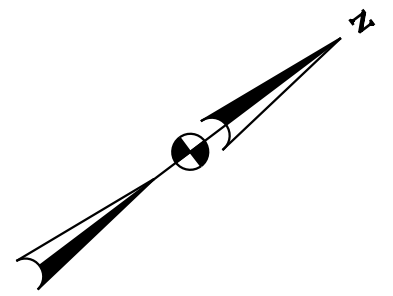
ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE

SIGNING AND MARKING PLAN	
REVISIONS	SCALE 1" = 15' DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. PS31
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN - 2.08 OF 11	SHEET NO. 23 OF 29

WRA
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: bdonoway



MATCH LINE STA. 26+17 - SEE SHEET SN-2.08

LIMIT OF WORK
CONTRACT NO. PGA115133
MD 725
STA. 28+10

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

PAVEMENT MARKING LEGEND	
(A)	5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
(B)	24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
(C)	REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

GENERAL NOTES:

- THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

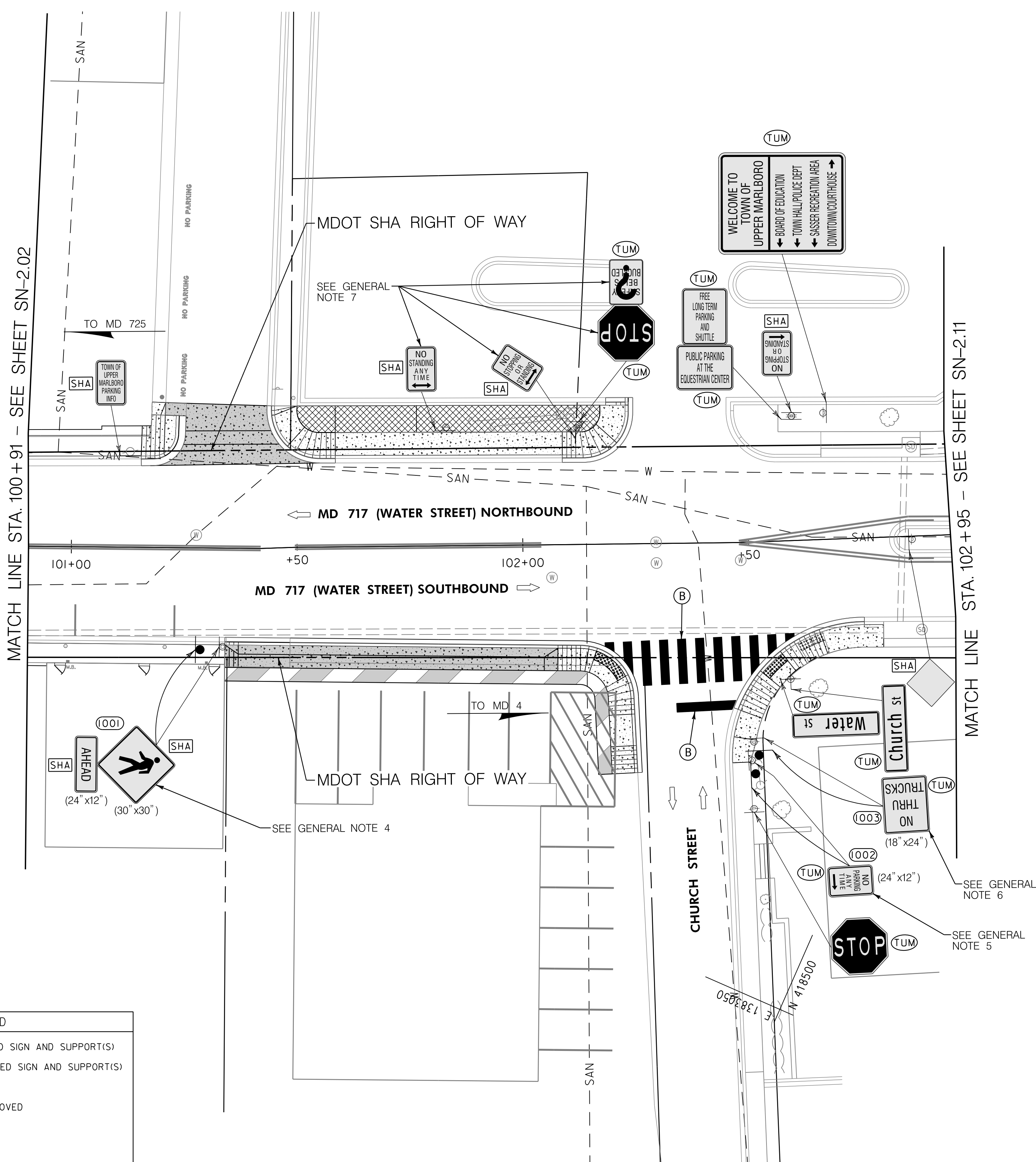
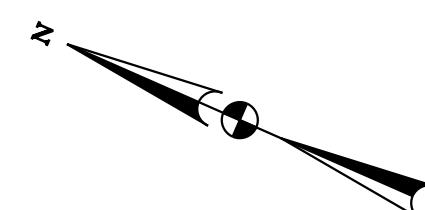
ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE

SIGNING AND MARKING PLAN	
SCALE	1" = 15'
DATE	APRIL 12, 2022
CONTRACT NO.	PGA115133
DESIGNED BY	JHR
COUNTY	PRINCE GEORGE'S
DRAWN BY	JHR
LOGMILE	16072500.59 - 16072501.13
CHECKED BY	JMM
TOD NO.	PS31
MDE/PRD	20-PB-0006-03
DRAWING NO.	SN - 2.09 OF 11
SHEET NO.	24 OF 29

WRA
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: bdonoway



GENERAL NOTES:

1. THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
2. CONTRACTOR SHALL CONTACT MR. DARNELL BOND, DIRECTOR OF PUBLIC WORKS FOR THE TOWN OF UPPER MARLBORO AT dbond@uppermarboromd.gov OR 301-509-4756 A MINIMUM OF 7 DAYS PRIOR TO REMOVAL OF SIGNS NOTED AS OWNED BY THE TOWN TO COORDINATE RETURN OF THE SIGNS TO THE TOWN.
3. SEE CONTINENTAL STRIPING DETAIL ON DRAWING NO. SN-2.04.
4. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "PEDESTRIAN AHEAD" SIGN TO STATION 101+28 RIGHT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 1' OFFSET FROM FACE OF CURB TO LEFT EDGE OF SIGN, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.
5. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "NO PARKING ANY TIME" SIGN 3' TO THE WEST ON CHURCH STREET AND LOCATED 5.67' FROM THE EXISTING FACE OF CURB ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.
6. REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "NO THRU TRUCKS" SIGN 3' TO THE WEST ON CHURCH STREET AND LOCATED 6.5' FROM THE EXISTING FACE OF CURB ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, MAINTAINING A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.
7. EXISTING SIGNS AND SIGN SUPPORTS TO REMAIN. ADJUST EXISTING SIGNS AND SUPPORTS AS NEEDED TO REMOVE SURROUNDING MATERIAL.
8. EXISTING SIDEWALK, INCLUDING AREAS OF BRICK, DISTURBED BY SIGN SUPPORT REMOVAL SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER.

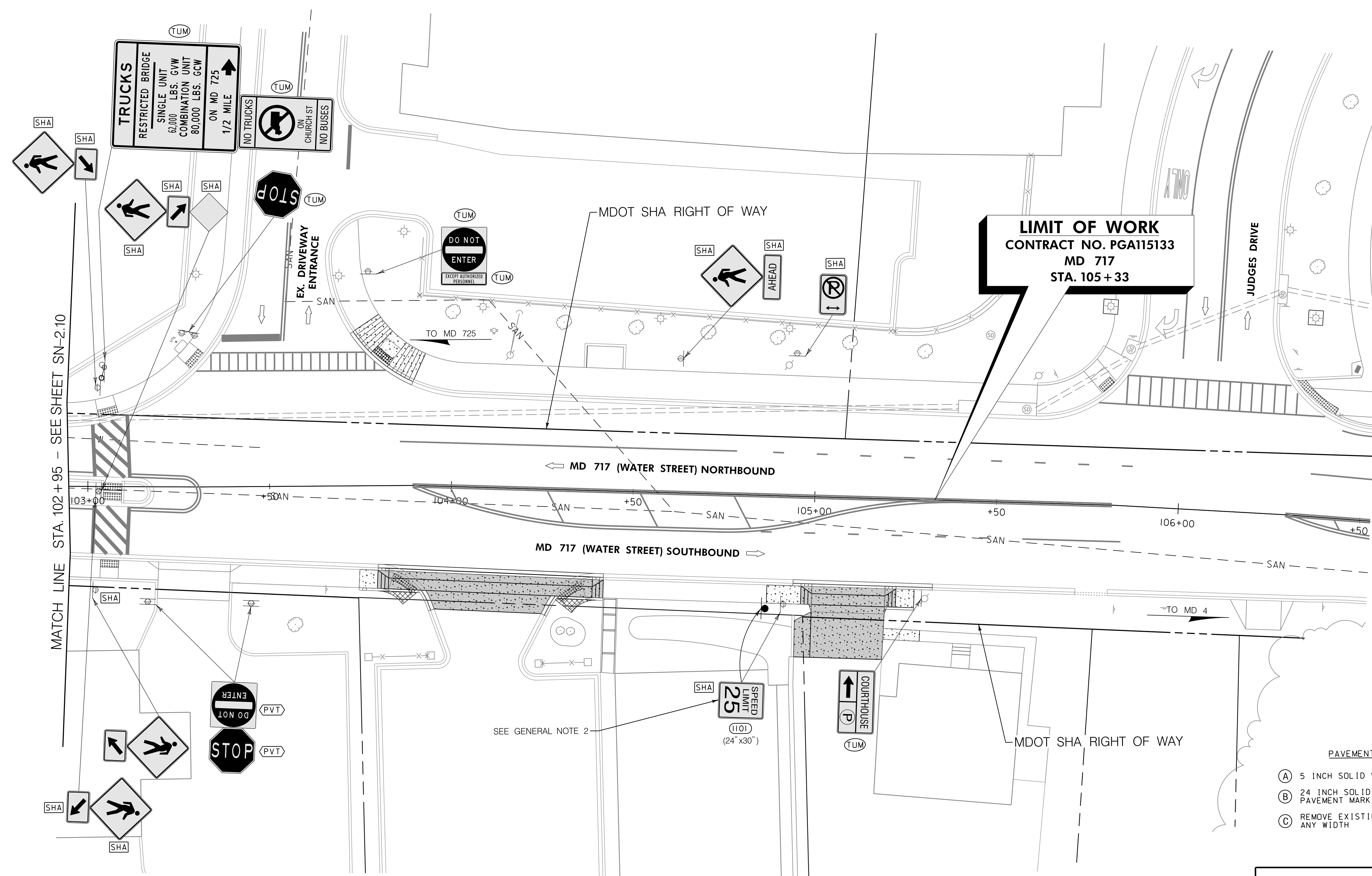
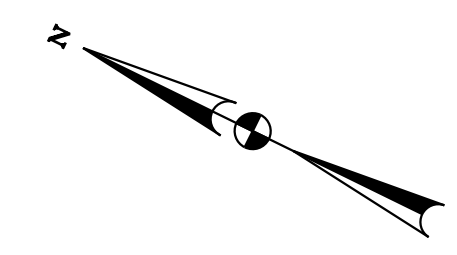
SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

PAVEMENT MARKING LEGEND	
	5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
	24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
	REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

SIGNING AND MARKING PLAN	
SCALE 1" = 15'	DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. PS31
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN-2.10 OF 11	SHEET NO. 25 OF 29

Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: jmocty



LIMIT OF WORK
CONTRACT NO. PGA115133
MD 717
STA. 105+33

MATCH LINE STA. 102+95 - SEE SHEET SN-2.10

- PAVEMENT MARKING LEGEND**
- (A) 5 INCH SOLID WHITE THERMOPLASTIC PAVEMENT MARKINGS
 - (B) 24 INCH SOLID WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKINGS
 - (C) REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH

SIGN LEGEND	
	EXISTING GROUND MOUNTED SIGN AND SUPPORT(S)
	PROPOSED GROUND MOUNTED SIGN AND SUPPORT(S)
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED
	TRAFFIC FLOW ARROWS
	SIGN NO.
FINAL OWNERSHIP AND MAINTENANCE RESPONSIBILITY:	
	MDOT SHA
	TOWN OF UPPER MARLBORO
	PRIVATE
	PRINCE GEORGES COUNTY

SEE GENERAL NOTE 2

- GENERAL NOTES:**
- THE FOLLOWING MDOT SHA TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATION (TTCTA) IS SUGGESTED FOR THE WORK SHOWN ON THIS PLAN: MD 104.02-10.
 - REMOVE EXISTING SIGN SUPPORT ONLY. RELOCATE EXISTING "SPEED LIMIT 25 MPH" SIGN TO STATION 104+87 RIGHT ON NEW SQUARE PERFORATED TUBULAR STEEL SIGN SUPPORT, OFFSET 7.167' FROM FACE OF CURB TO LEFT EDGE OF SIGN, AND A MINIMUM 7' FROM TOP OF SIDEWALK TO BOTTOM OF SIGN.

E 1383.100
N 418.300

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE

SIGNING AND MARKING PLAN	
SCALE 1" = 15'	DATE APRIL 12, 2022 CONTRACT NO. PGA115133
DESIGNED BY JHR	COUNTY PRINCE GEORGE'S
DRAWN BY JHR	LOGMILE 16072500.59 - 16072501.13
CHECKED BY JMM	TMS NO. P531
MDE/PRD 20-PR-0006-03	TOD NO.
DRAWING NO. SN-2.11	OF 11 SHEET NO. 26 OF 29

WRA
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

SHEET NO.	SIGN NO.	REMARKS	CODE NUMBERS •														
			1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
SN-2.01		CODE NO. 4					17.875										
SN-2.02		CODE NO. 4					33.75										
SN-2.03	301	RELOCATE EX. *NO U-TURN* SIGN (24" x 24")		1	1	4											
	302	RELOCATE EX. *PEDESTRIAN* & *AHEAD* SIGNS (30" x 30") & (24" x 12")		1	1	8.25											
	303	RELOCATE EX. *NO PARKING POLICE PARKING ONLY* & *VIOLATORS WILL BE TOWED* SIGNS (12" x 18") & (12" x 6")		1	1	2											
		CODE NO. 4					10										
SN-2.04	401	RI-6a(3)(L) (30" x 30")	6.25	1	1												
	402	RI-6a(3)(L) (30" x 30")	6.25	1	1												
		CODE NO. 4					35.875										
SN-2.06	601	RELOCATE EX. *SPEED LIMIT 25 MPH* SIGN (24" x 30")		1	1	5											
	602	RELOCATE EX. *NO PARKING* SIGN (12" x 18")		1	1	1.5											
SN-2.07	701	RELOCATE EX. *NO PARKING* SIGN (12" x 18")		1	1	1.5											
SN-2.10	1001	RELOCATE EX. *PEDESTRIAN* & *AHEAD* SIGNS (30" x 30") & (24" x 12")		1	1	8.25											
	1002	RELOCATE EX. *NO PARKING ANY TIME* SIGN (12" x 18")		1	1	1.5											
	1003	RELOCATE EX. *NO THRU TRUCKS* SIGN (18" x 24")		1	1	3											
SN-2.11	1101	RELOCATE EX. *SPEED LIMIT 25 MPH* SIGN (24" x 30")		1	1	5											
PAVEMENT MARKINGS																	
SN-2.04										85	115						
SN-2.09										15	10						
SN-2.10										100							
TOTAL QUANTITIES			12.5	12	12	40	97.5	15	185	125							
PROPOSED QUANTITIES			15	13	13	45	105	20	205	140							



• CODE NUMBER DESCRIPTION & UNIT					
CODE NUMBERS	DESCRIPTION	UNIT	CODE NUMBERS	DESCRIPTION	UNIT
1	SHEET ALUMINUM SIGNS	SF	9		
2	SQUARE PERFORATED TUBULAR STEEL SIGN POSTS	EA	10		
3	SQUARE TUBULAR STEEL ANCHOR BASES	EA	11		
4	RELOCATE EXISTING GROUND MOUNTED SIGNS	SF	12		
5	REMOVE EXISTING GROUND MOUNTED SIGNS & SUPPORTS	SF	13		
6	5" WHITE THERMOPLASTIC PAVEMENT MARKINGS	LF	14		
7	24" WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LINES	LF	15		
8	REMOVAL OF EXISTING PAVEMENT MARKING LINES, ANY WIDTH	LF			

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

ADA SIDEWALK UPGRADES

MD 725 (MAIN STREET) FROM WEST OF SERVICE LANE TO EAST OF GOVERNOR ODEN BOWIE DRIVE

REVISIONS	INDEX OF QUANTITIES
	SCALE N.T.S. DATE APRIL 12, 2022 CONTRACT NO. PG15133
	DESIGNED BY JHR COUNTY PRINCE GEORGE'S
	DRAWN BY JHR LOGMILE 16072500.59 - 16072501.13
	CHECKED BY JMM TMS NO. PS31
	MDE/PRD 20-PR-0006-03 TOD NO.
	DRAWING NO. SN-11.01 OF 1 SHEET NO. 27 OF 29

BY: jmoony

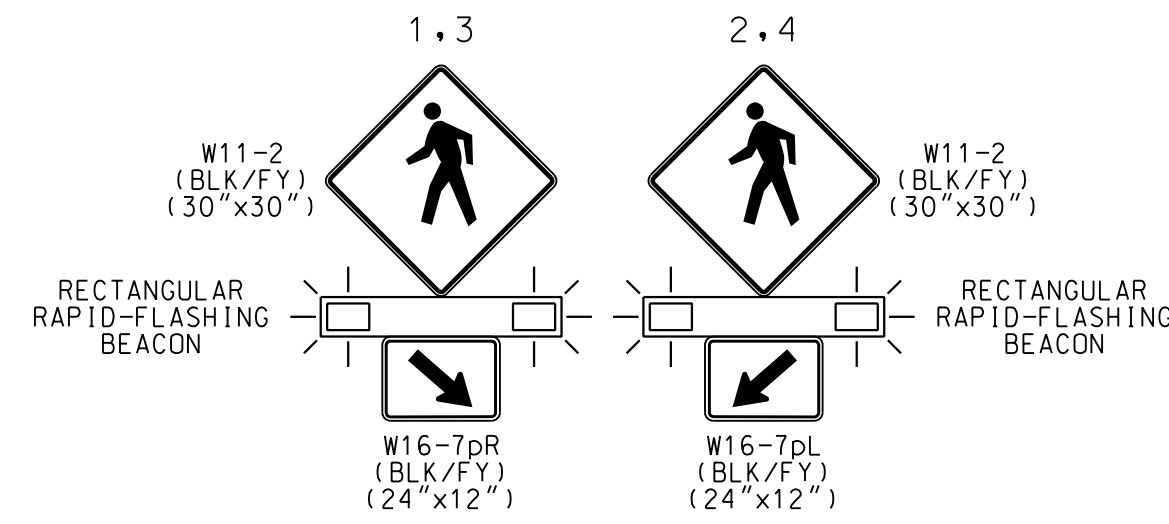


MD 725 IS ASSUMED TO RUN IN AN EAST-WEST DIRECTION

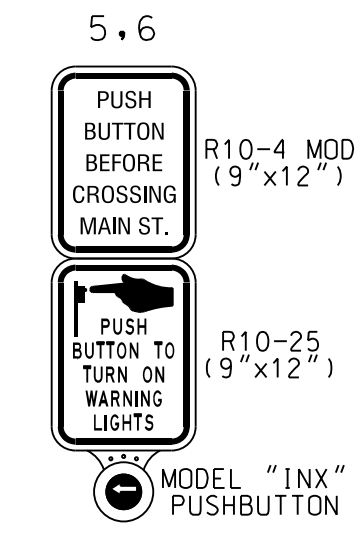
CONSTRUCTION DETAILS

- A. INSTALL CONCRETE FOUNDATION WITH 14 FT. STEEL PEDESTAL POLE WITH BREAKAWAY TRANSFORMER BASE, POLE MOUNTED CONTROLLER CABINET, SOLAR POWER EQUIPMENT, RECTANGULAR RAPID-FLASHING BEACON, SIGNS, AND PEDESTRIAN PUSHBUTTON AND R10-4MOD/R10-25 SIGNS. (SEE INSTALLATION DETAIL ON DWG. NO. SG-02) (INSTALL 1-3 IN. SCHEDULE 80, 90 DEGREE PVC ELECTRICAL CONDUIT BENDS IN PEDESTAL BASE).
- B. INSTALL CONCRETE FOUNDATION WITH 14 FT. STEEL PEDESTAL POLE WITH BREAKAWAY TRANSFORMER BASE, POLE MOUNTED CONTROLLER CABINET, CELLULAR ANTENNA AND LEAD-IN CABLE, SOLAR POWER EQUIPMENT, RECTANGULAR RAPID-FLASHING BEACON, SIGNS, AND PEDESTRIAN PUSHBUTTON AND R10-4 MOD/R10-25 SIGNS. (SEE INSTALLATION DETAIL ON DWG. NO. SG-02) (INSTALL 1-3 IN. SCHEDULE 80, 90 DEGREE PVC ELECTRICAL CONDUIT BENDS IN PEDESTAL BASE).
- C. INSTALL HANDHOLE WITH GROUND ROD.
- D. INSTALL 3 INCH CONDUIT- TRENCHED. INSTALL NO. 6 AWG STRANDED BARE COPPER GROUND WIRE FROM EMBEDDED GROUND ROD TO PEDESTAL POLE GROUND LUG.
- E. INSTALL 24 IN. WHITE THERMOPLASTIC PAVEMENT MARKING FOR CROSSWALKS.
- F. INSTALL 24 IN. WHITE THERMOPLASTIC PAVEMENT MARKING FOR STOP LINE.
- G. REMOVE EXISTING PAVEMENT MARKING LINES, ANY WIDTH.

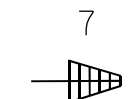
PROPOSED RECTANGULAR RAPID-FLASHING (RRFB) BEACON SIGNS



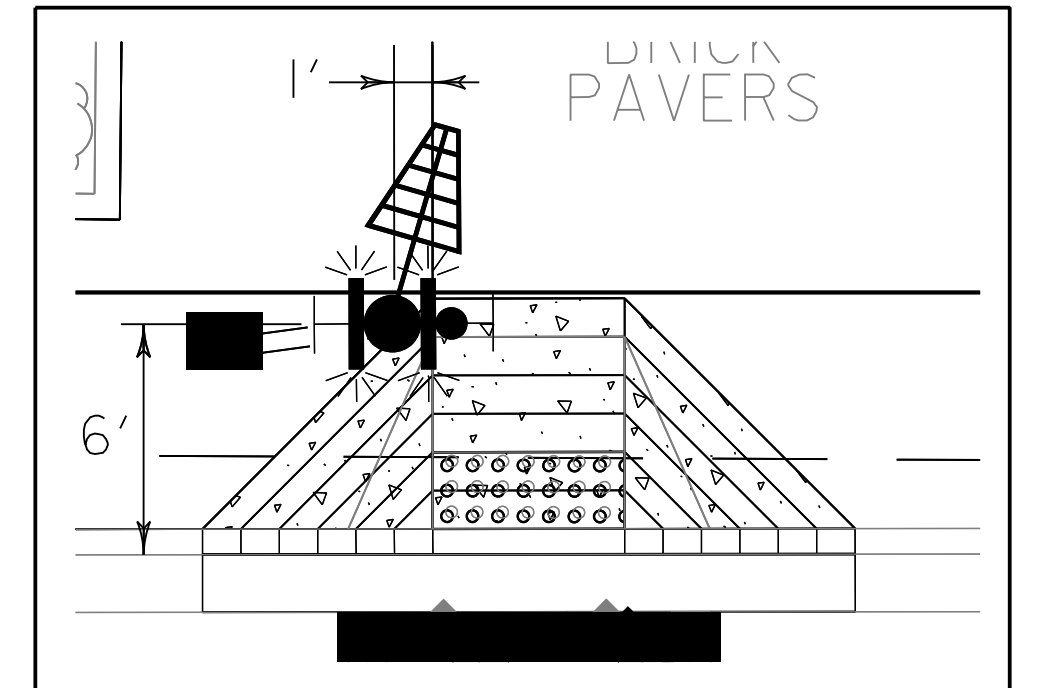
PROPOSED PUSHBUTTON SIGNS



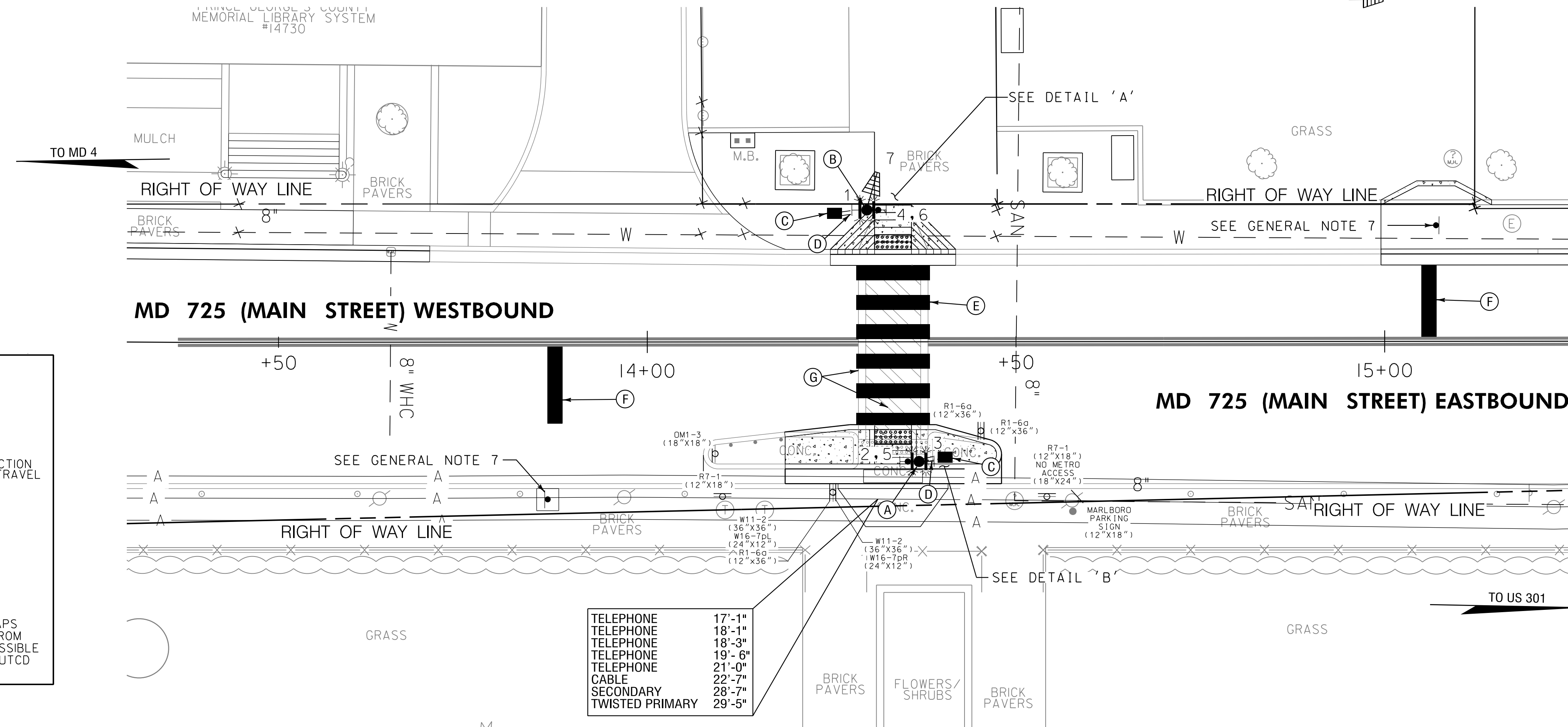
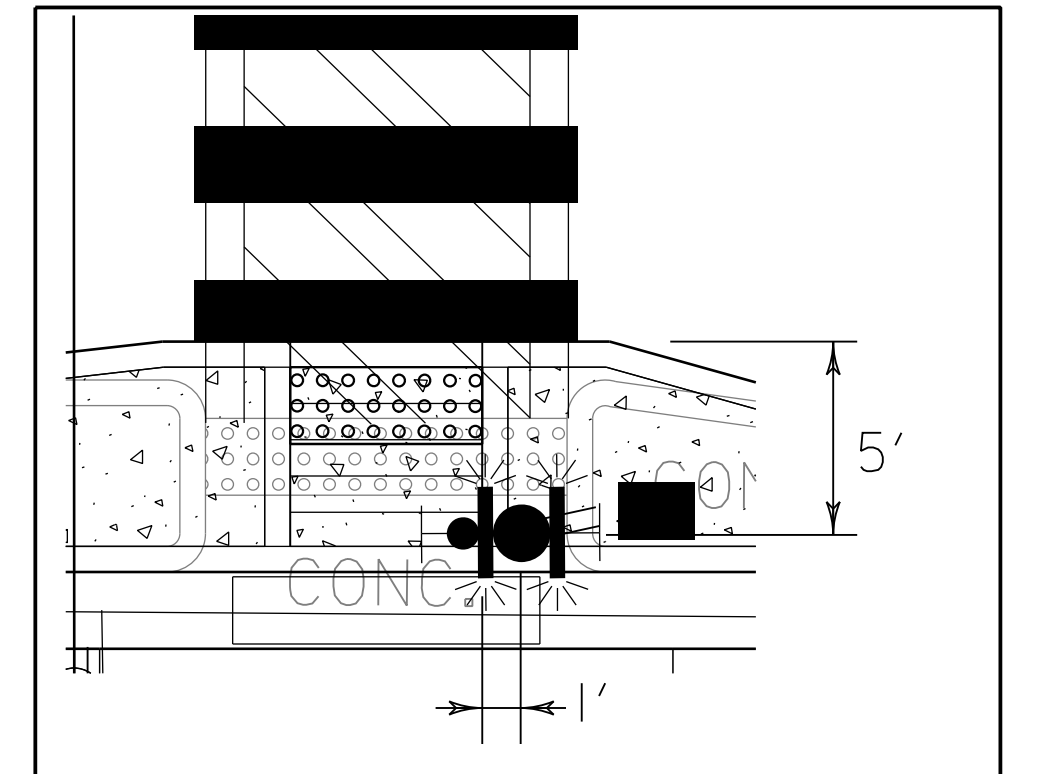
PROPOSED CELLULAR ANTENNA



DIMENSION DETAIL 'A'
SCALE: 1"=5'



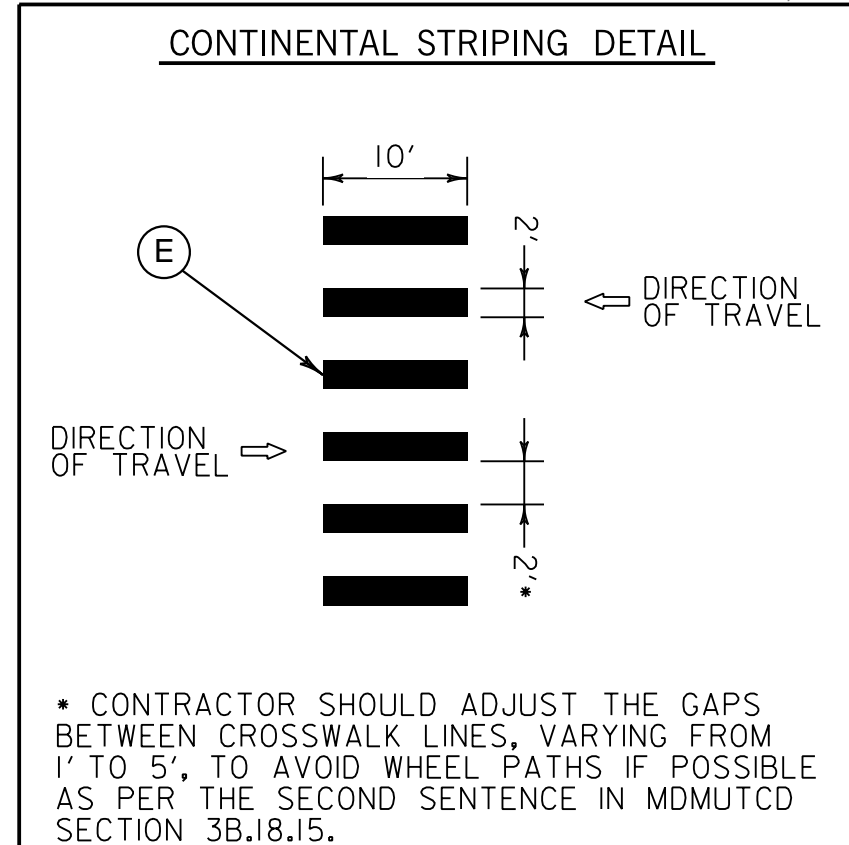
DIMENSION DETAIL 'B'
SCALE: 1"=5'



SPECIAL NOTE:
CONTRACTOR SHALL USE CAUTION WHEN INSTALLING 14 FT. PEDESTAL POLE TO AVOID DISTURBANCE OF EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL TEST PIT TO DETERMINE EXACT LOCATION AND DEPTH OF UNDERGROUND UTILITIES PRIOR TO INSTALLING SIGNAL EQUIPMENT. CONTRACTOR SHALL EXCAVATE FOUNDATION BY HAND IN ORDER TO MINIMIZE POSSIBLE DAMAGE TO EXISTING UNDERGROUND UTILITIES.

GENERAL NOTES

1. ALL TRAFFIC SIGNAL FOUNDATIONS SHALL BE INSTALLED AT THE FINAL SIDEWALK OR CURB GRADE FOR CLOSED SECTIONS, HIGHEST ROADWAY PROFILE GRADE FOR OPEN SECTIONS, TO MEET CLEARANCES AS SPECIFIED IN MD 816.03, MD 818.01, MD 818.02, MD 818.04. THE CONTRACTOR SHALL VERIFY ULTIMATE GRADES PRIOR TO THE INSTALLATION OF ALL SIGNAL EQUIPMENT.
2. THE CONTRACTOR SHALL VERIFY ALL UNDERGROUND UTILITIES PRIOR TO INSTALLING PROPOSED SIGNAL EQUIPMENT. IF ANY UTILITY CONFLICTS SHOULD ARISE THE CONTRACTOR SHALL CONTACT THE PROJECT ENGINEER.
3. VERIFY PROPOSED GEOMETRICS PRIOR TO INSTALLING SIGNAL EQUIPMENT.
4. REFER TO ROADWAY PLANS FOR SIDEWALK RAMPS DETAILS.
5. PUSHBUTTONS ARE TO BE LOCATED SO THAT THEY CAN BE ACTIVATED BY A PERSON IN A WHEELCHAIR REACHING LESS THAN 18" FROM A 60" x 60" LEVEL LANDING AREA WITH A CROSS SLOPE OF LESS THAN OR EQUAL TO 2%.
6. LOCATION OF ACCESSIBLE PEDESTRIAN SIGNAL PUSHBUTTONS MUST MEET LOCATION REQUIREMENTS OF MUTCD SEC. 4E.08 AND 4E.10 AND FIG. 4E-3 AND 4E-4 OF THE NCHRP PUBLICATION, "ACCESSIBLE PEDESTRIAN SIGNALS: GUIDE TO BEST PRACTICE". IF NOT MET, THE CONTRACTOR IS TO STOP WORK ON PUSHBUTTON LOCATIONS UNTIL A DESIGN WAIVER IS OBTAINED, APPROVED BY THE DIRECTOR, OFFICE OF TRAFFIC AND SAFETY.
7. REFER TO SIGNING AND MARKING PLANS FOR SIGN INSTALLATION DETAILS.



OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION

STATE HIGHWAY ADMINISTRATION

RECTANGULAR RAPID-FLASHING BEACON
MD 725 (MAIN STREET) AT
PRINCE GEORGES COUNTY
CIRCUIT COURTHOUSE
UPPER MARLBORO, MD

GEOMETRIC LEGEND	APPROVALS	REVISIONS
EXISTING		
PROPOSED		
UTILITY LEGEND		
SD - STORM DRAIN		
G - GAS MAIN		
W - WATER MAIN		
S - SEWER MAIN		
E - ELECTRIC CABLES		
A - AERIAL CABLES		
T - TELEPHONE CABLES		
F - FIBER-OPTIC		

APPROVALS	REVISIONS
Robert Stewart 01/11/22 TEAM LEADER	
Adison Jorotic 1/12/2022 ASST. ENV. CHIEF	
Qualin Tian 2/1/2022 DRAWING CHIEF	
Cedric Ward 2/2/22 OFFICE DIRECTOR	

TRAFFIC SIGNALIZATION PLAN
SCALE 1" = 10' DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
DESIGNED BY B. DONOWAY COUNTY PRINCE GEORGES
DRAWN BY B. DONOWAY LOGMILE 16072500.86
CHECKED BY J. MOCNY TMS NO. PS31
MDE/PRD SEE TITLE SHEET TOD NO.
TS NO. 5235 DRAWING SG- 01 OF 2 SHEET NO. 28 OF 29

WRA
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

PROJECT DESCRIPTION

GENERAL

THIS PORTION OF THE PROJECT INVOLVES THE INSTALLATION OF PEDESTRIAN RECTANGULAR RAPID-FLASHING BEACONS (RRFB) AT THE PEDESTRIAN CROSSING ON MD 725 (MAIN STREET) AND THE COURTHOUSE MID-BLOCK CROSSING LOCATED BETWEEN W. AND E. COURT DRIVES IN PRINCE GEORGE'S COUNTY, MARYLAND. MD 725 IS ASSUMED TO RUN IN AN EAST-WEST DIRECTION.

INTERSECTION OPERATION

APS MESSAGE FOR CROSSING MD 725 (MAIN STREET):

A. WHEN A PEDESTRIAN LOCATES AND PRESSES PUSHBUTTON FOR AN EXTENDED TIME, THE PUSHBUTTON UNIT WILL ANNOUNCE "YELLOW LIGHTS ARE FLASHING FOR CROSSING MAIN STREET. WATCH FOR TRAFFIC. USE CAUTION." AUDIBLE MESSAGE SHALL BE SPOKEN TWICE PER ACTIVATION.

VIBROTACTILE OR PERCUSSIVE INDICATIONS SHALL NOT BE USED IN CONJUNCTION WITH AUDIBLE PUSHBUTTON INFORMATION MESSAGE.

CONTROLLER REQUIREMENTS

RAPID RECTANGULAR FLASHING BEACON (POLE MOUNTED CONTROLLER CABINET AND SOLAR PANEL) ASSEMBLY, WITH COMMUNICATIONS.

EQUIPMENT LIST "A"

A. EQUIPMENT TO BE SUPPLIED BY THE MDOT SHA

ITEM NO.	QUANTITY	DESCRIPTION
973025	36 S.F.	SHEET ALUMINUM MAST ARM/POLE MOUNTED SIGN: - 2 EACH R10-4 MOD (9 IN. x 12 IN.) - POLE MOUNT - 2 EACH R10-25 (9 IN. x 12 IN.) - POLE MOUNT - 4 EACH W11-2 (BLK/FY) (30 IN. x 30 IN.) - POLE MOUNT - 2 EACH W16-7pL (BLK/FY) (24 IN. x 12 IN.) - POLE MOUNT - 2 EACH W16-7pR (BLK/FY) (24 IN. x 12 IN.) - POLE MOUNT
974100	1 EACH	CELLULAR MODEM, ANTENNA AND LEAD-IN CABLE

EQUIPMENT LIST "B"

B. EQUIPMENT TO BE FURNISHED AND/OR INSTALLED BY THE CONTRACTOR

ITEM NO.	QUANTITY	DESCRIPTION
203030	2 C.Y.	TEST PIT EXCAVATION
549800	115 L.F.	REMOVAL OF EXISTING PAVEMENT MARKING LINES, ANY WIDTH
549419	82 L.F.	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS
800000	1 L.S.	RECTANGULAR RAPID-FLASHING BEACONS ASSEMBLY
800000	20 L.F.	ELECTRICAL CABLE - 8 CONDUCTOR (NO. 18 AWG) FOR MODEL INX APS PUSHBUTTON
800000	2 EACH	AUDIBLE/TACTILE PEDESTRIAN MODEL INX PUSHBUTTON STATION WITH LED AND SIGN
801004	1.4 C.Y.	CONCRETE FOR SIGNAL FOUNDATION
802501	30 L.F.	NO. 6 AWG STRANDED BARE COPPER GROUND WIRE
805135	20 L.F.	3 INCH SCHEDULE 80 RIGID PVC CONDUIT - TRENCHED
811001	2 EA	FURNISH AND INSTALL ELECTRICAL HANDHOLE
813015	36 S.F.	INSTALL OVERHEAD SIGN
815425	1 EACH	INSTALL CELLULAR ANTENNA AND LEAD-IN CABLE
818010	2 EACH	14 FOOT BREAKAWAY PEDESTAL POLE
837001	2 EACH	GROUND ROD - 3/4 INCH DIAMETER X 10 FOOT LENGTH

MAINTENANCE OF TRAFFIC

THE FOLLOWING TRAFFIC CONTROL STANDARDS SHALL BE REFERENCED FOR THE PROJECT. ADDITIONAL TRAFFIC CONTROL STANDARDS MAY BE USED AS DIRECTED BY THE ENGINEER.

STANDARD NO. MD-104.02-02 (SHOULDER WORK)

STANDARD NO. MD-104.02-10 (FLAGGING OPERATION)

STANDARD NO. MD-104.06-10 (MOBILE SERVICE WORK/INTERSECTION)

EQUIPMENT LIST "C"

NO EQUIPMENT TO BE REMOVED AND RETURNED TO MDOT SHA

PROJECT CONTACTS

THE CONTACT PERSONS FOR MDOT SHA ARE AS FOLLOWS:

DISTRICT 3 - PRINCE GEORGES CO.

MS. ERICA RIGBY
DISTRICT ENGINEER
PHONE: (301) 513-7346

MR. DEREK GUNN
DEPUTY DISTRICT ENGINEER
PHONE: (301) 513-7498

MR. JOHN GOVER (FIELD)
PHONE: (301) 513-7336
ASSISTANT DISTRICT ENGINEER CONSTRUCTION

MS. AMY ANDREWS (ADMIN.)
ASSISTANT DISTRICT ENGINEER CONSTRUCTION
PHONE: (301) 513-7300

MR. JUSTIN SOSEBEE
ASSISTANT DISTRICT ENGINEER - MAINTENANCE (PRINCE GEORGES)
PHONE: (301) 776-7619

MR. MARK LOEFFLER
DISTRICT UTILITY ENGINEER
PHONE: (301) 513-7350

PETER CAMPANIDES, P.E.
ASSISTANT DISTRICT ENGINEER - TRAFFIC (PRINCE GEORGES)
PHONE: (301) 513-7409

OFFICE OF TRAFFIC AND SAFETY

VIVIAN BERRA-FIGUEROA, P.E.
DEPUTY DIRECTOR, OFFICE OF TRAFFIC AND SAFETY
PHONE: (410) 787-7630

REBECCA LICHTENSTEIN, P.E.
ASSISTANT DIVISION CHIEF
TRAFFIC OPERATIONS DIVISION
PHONE: (410) 787-7625

MR. MICHAEL BASSO
CHIEF, SIGNAL OPERATIONS
PHONE: (410)-787-7652

MR. DAVID (TODD) JONES
MANAGER, SHA SIGN OPERATIONS
PHONE: (410) 787-7676

MR. MICHAEL BOYLE
ODTS, SIGNAL WAREHOUSE
PHONE: (410) 787-7673

PHASE CHART

	1	2	3	4	
REST	DARK	DARK	DARK	DARK	
PED ACTIVATED RAPID FLASHING	RFL	RFL	RFL	RFL	⋮

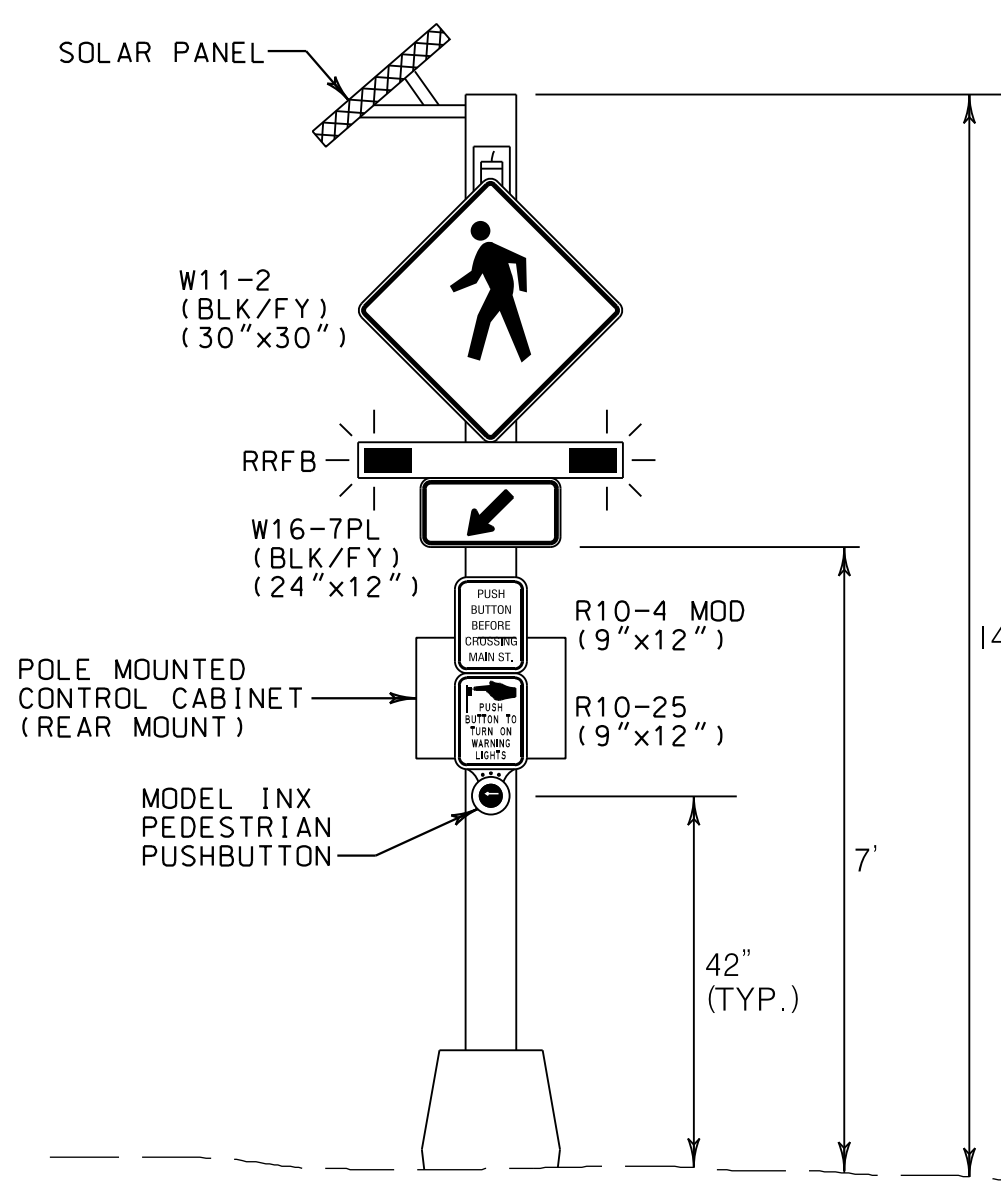
RFL = RAPID FLASH (SEE RRFB FLASHING NOTES)

EXAMPLE RRFB INSTALLATION DETAILS

(SEE DWG. NO. SG-01 FOR SIGNS/RRFB/PUSHBUTTON PLACEMENT)

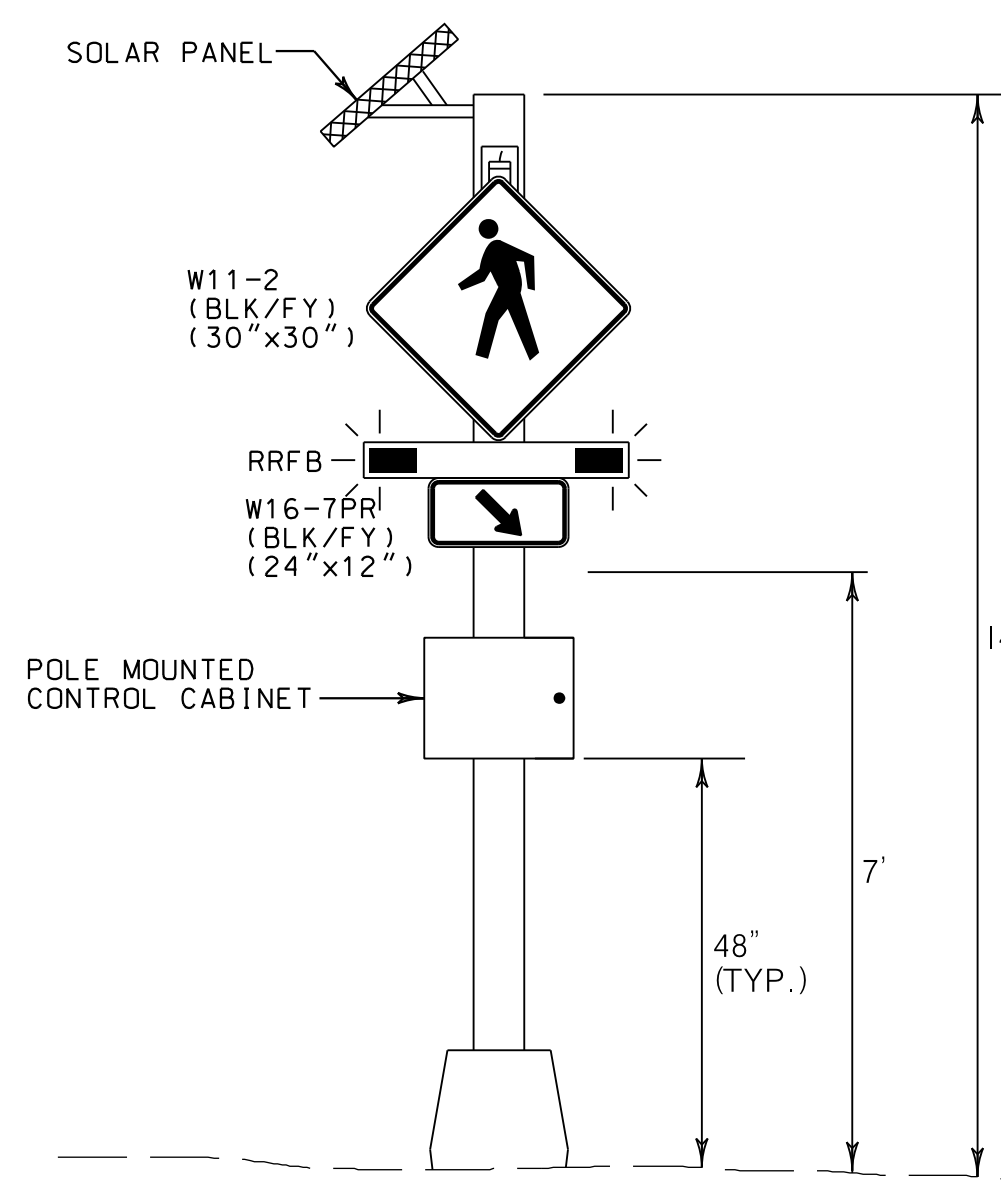
EXAMPLE FRONT VIEW

(REAR VIEW EQUIPMENT NOT SHOWN FOR CLARITY)

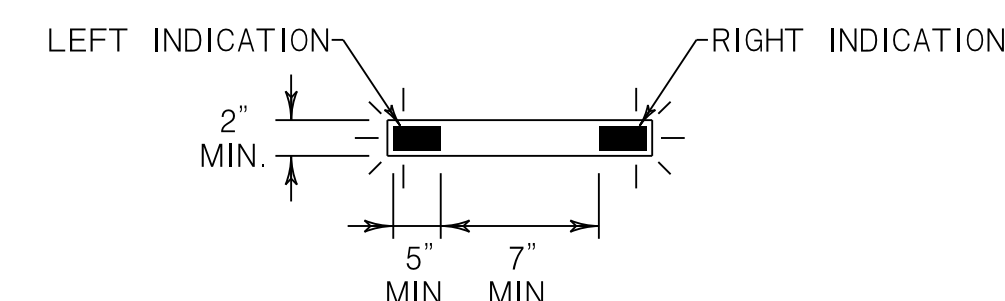


EXAMPLE REAR VIEW

(FRONT VIEW EQUIPMENT NOT SHOWN FOR CLARITY)



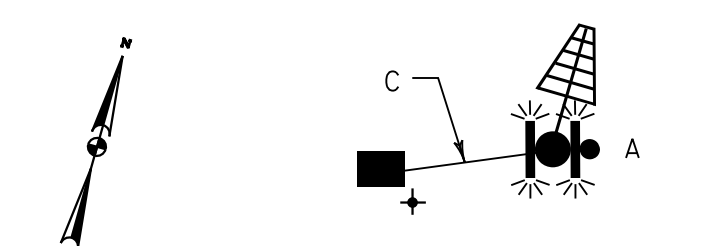
RRFB UNIT DETAIL



NOTE:

TOTAL WIDTH OF HOUSING SHALL NOT PROJECT BEYOND THE OUTSIDE EDGES OF THE W11-2 SIGN AND SUPPLEMENTS.

WIRING DIAGRAM



RRFB FLASHING NOTES

ALL FLASHING UNITS AT THE CROSSING SHALL OPERATE IN UNISON UPON ACTIVATION.

FLASHING OPERATION SHALL CONFORM TO THE REQUIREMENTS OF FHWA 1A-21.

RRFB SHALL PROVIDE 75 FLASHING SEQUENCES PER MINUTE USING THE FOLLOWING 800-MILLISECOND FLASHING SEQUENCE. DURING EACH 800-MILLISECOND FLASHING SEQUENCE, THE LEFT AND RIGHT RRFB INDICATIONS SHALL OPERATE USING THE FOLLOWING SEQUENCE:

1. THE RRFB INDICATION ON THE LEFT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
2. BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
3. THE RRFB INDICATION ON THE RIGHT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
4. BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
5. THE RRFB INDICATION ON THE LEFT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
6. BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
7. THE RRFB INDICATION ON THE RIGHT-HAND SIDE SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
8. BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
9. BOTH RRFB INDICATIONS SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
10. BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 50 MILLISECONDS.
11. BOTH RRFB INDICATIONS SHALL BE ILLUMINATED FOR APPROXIMATELY 50 MILLISECONDS.
12. BOTH RRFB INDICATIONS SHALL BE DARK FOR APPROXIMATELY 250 MILLISECONDS.



Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

BY: jmochny

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

RECTANGULAR RAPID-FLASHING BEACON
MD 725 (MAIN STREET) AT
PRINCE GEORGES COUNTY
COURTHOUSE
UPPER MARLBORO, MD

REVISIONS	GENERAL INFORMATION SHEET	
	SCALE N.T.S.	DATE APRIL 12, 2022 CONTRACT NO. PGAT15133
	DESIGNED BY B. DONOWAY	COUNTY PRINCE GEORGES
	DRAWN BY B. DONOWAY	LOGMILE 16072500.86
	CHECKED BY J. MOCHNY	TMS NO. P531
	MDE/PRD SEE TITLE SHEET	TOD NO.
	TS NO. 5235 DRAWING SG- 02 OF 2	SHEET NO. 29 OF 29